



Placentia Planning Commission Agenda

Regular Meeting

May 13, 2025

6:30 p.m.

City Council Chambers

401 E. Chapman Avenue, Placentia, CA

Frank Perez
Chair

Matthew Roche
Vice Chair

Brandon Evans
Commissioner

Juan Guerrero
Commissioner

Thomas Ingalls
Commissioner

Juan Navarro
Commissioner

David Smith
Commissioner

City of Placentia
401 E Chapman Avenue
Placentia, CA 92870

Phone: (714) 993-8124
Fax: (714) 528-4640
Website: www.placentia.org

Procedures for Addressing the Commission

Any person who wishes to speak regarding an item on the agenda or on a subject within the Planning Commission's jurisdiction during the "Oral Communications" portion of the agenda should fill out a "Speaker Request Form" and give it to the Commission Secretary BEFORE that portion of the agenda is called. Testimony for Public Hearings will only be taken at the time of the hearing.

The Commission encourages free expression of all points of view. To allow all persons the opportunity to speak, please keep your remarks brief. If others have already expressed your position, you may simply indicate that you agree with a previous speaker. If appropriate, a spokesperson may present the views of an entire group. To encourage all views, the Commission discourages clapping, booing or shouts of approval or disagreement from the audience.

PLEASE SILENCE CELL PHONES AND OTHER ELECTRONIC EQUIPMENT WHILE THE COMMISSION IS IN SESSION.

Special Accommodations

In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Clerk's Office at (714) 993-8231. Notification 48 hours prior to the meeting will generally enable City staff to make reasonable arrangements to ensure accessibility. (28 CFR 35.102.35.104 ADA Title II)

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In compliance with California Government Code Section 54957.5, any writings or documents provided to a majority of the Planning Commission regarding any item on this agenda that are not exempt from disclosure under the Public Records Act will be made available for public inspection at the City Clerk's Office at City Hall, 401 East Chapman Avenue, Placentia, during normal business hours.

Study Sessions are open to the public and held in the City Council Chambers or City Hall Community Room.

REGULAR MEETING
6:30 p.m. – City Council Chambers

CALL TO ORDER:

ROLL CALL: Commissioner Evans
Commissioner Guerrero
Commissioner Ingalls
Commissioner Navarro
Commissioner Smith
Vice Chair Rocke
Chair Perez

PLEDGE OF ALLEGIANCE:

ORAL COMMUNICATIONS:

At this time the public may address the Planning Commission concerning any agenda item, which is not a public hearing item, or on matters within the jurisdiction of the Planning Commission. There is a five (5) minute time limit for each individual addressing the Planning Commission.

CONSENT CALENDAR:

1. **Planning Commission Regular Meeting Minutes – March 11, 2025**

RECOMMENDATION: Approve

REGULAR AGENDA:

1. **Applicant:** City of Placentia

Project Location: Citywide

**GENERAL PLAN CONFORMANCE FINDING FOR FISCAL YEAR 2025-26
CAPITAL IMPROVEMENT PROGRAM**

RECOMMENDATION:

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PLACENTIA, CALIFORNIA, FINDING THAT THE FISCAL YEAR 2025-26 CAPITAL IMPROVEMENT PROGRAM IS EXEMPT FROM THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) PURSUANT TO CEQA GUIDELINES SECTION 15061 (B)(3), AND FINDING THAT THE FISCAL YEAR 2025-26 CAPITAL IMPROVEMENT PROGRAM CONFORMS TO THE ADOPTED GENERAL PLAN AND RECOMMENDING THE CITY COUNCIL APPROVE THE CAPITAL IMPROVEMENT PROGRAM

2. **Applicant:** City of Placentia

Project Location: Citywide

**STUDY SESSION TO REVIEW CHAPMAN CORRIDOR REVITALIZATION
PLAN PROGRESS**

Recommended Action: It is recommended that the Planning Commission receive and file the attached City Council Staff Report and staff presentation and ask any questions of staff as appropriate.

PUBLIC HEARINGS: None

OLD BUSINESS:

NEW BUSINESS:

DEVELOPMENT REPORT:

DIRECTOR'S REPORT:

PLANNING COMMISSION REQUESTS

Commission members may make requests or ask questions of Staff. If a Commission member would like to have formal action taken on a requested matter, it will be placed on a future Commission Agenda.

ADJOURNMENT

The Planning Commissioners ADJOURN to the next regular meeting on June 10, at 6:30 p.m. in the City Council Chambers located at 401 East Chapman Avenue, Placentia CA, 92870.

CERTIFICATION OF POSTING

I, Joseph M. Lambert, Secretary to the Planning Commission of the City of Placentia, hereby certify that the Agenda for May 13, 2025, Regular Meeting of the Planning Commission of the City of Placentia was posted on May 8, 2025.



Joseph M. Lambert, Secretary

**PLACENTIA PLANNING COMMISSION
MINUTES
REGULAR MEETING
March 11, 2025
6:30 p.m. – City Council Chambers
401 E. Chapman Avenue, Placentia, CA**

CALL TO ORDER: Commissioner/Chair Perez called the meeting to order at 6:30 p.m.

ROLL CALL:

PRESENT: Commissioner Evans, Ingalls, Guerrero, Navarro, Rocke, Perez
ABSENT: None

STAFF PRESENT:

Assistant City Attorney Kristi J. Smith, Director of Development Services Joseph Lambert, Planning Manager Andrew Gonzales, Associate Planner Lesley Whittaker, Administrative Assistant Margie McCoy

PLEDGE OF ALLEGIANCE: Commissioner Evans

ORAL COMMUNICATIONS: None

CONSENT CALENDAR:

1. **Minutes
Placentia Planning Commission Regular Meeting of:
February 11, 2025**

Recommended Action: Approve
(Approved 5-0-1-0 as recommended)

Motion by Rocke second by Guerrero and carried on a (5-0-1-0) vote to approve the Consent Calendar.

Ayes: Guerrero, Ingalls, Navarro, Rocke, Perez
Noes: None
Abstain: Evans
Absent: None

PUBLIC HEARING:

1. **Applicant: Carla Hutchinson DBA No Limits Creative Arts**

Project Location: 350 S. Highland Avenue (Use Permit No. 2024-09)

USE PERMIT NO. 2024-09. A REQUEST TO PERMIT THE ESTABLISHMENT AND OPERATION OF A CREATIVE ART STUDIO FOR SPECIAL NEEDS STUDENTS, INCLUDING DANCE, VOICE, YOGA, AND ART INSTRUCTION CLASSES, WITHIN AN EXISTING 1,930-SQUARE-FOOT TENANT SPACE LOCATED AT 350 S. HIGHLAND AVENUE, UNIT B, WITHIN THE COBBLESTONE SHOPPING CENTER, LOCATED AT E. ORANGETHORPE AVENUE AND LAKEVIEW AVENUE.

Recommended Action: Adopt Resolution approving the project as recommended by Staff.

Chair Perez opened the Public Hearing at 6:32 p.m.

Director Lambert introduced Associate Planner Lesley Whittaker to present the Staff Report. Ms. Whittaker presented details of the proposed Use Permit for a Creative Arts Studio, including location, hours of operation, number of staff and number of weekly classes anticipated. Parking requirements are met because the shopping center has a surplus of 47 parking spaces. The studio will offer dance, voice, yoga, and art instruction classes for special needs students. The proposed space requires minor modifications, including partition walls, to create a large studio, reception area, office, lounge, kitchenette, and ADA compliant bathroom. Use Permit 2024-09 has been routed to various City departments, which have no concerns. Public Notices were mailed, posted and published.

The applicant, Carla Hutchinson, founder of No Limits Creative Arts, explained the program's origins and its growth to 90 students, emphasizing the need for more space to offer more classes and mentioned the program's inclusive environment.

Commissioners Guerrero and Rocke asked questions about the program's growth and accessibility issues.

Public comments from Jeff Roche of Stafford Circle and Kim Derek of Placentia in support of the Use Permit, highlighting its benefits for special needs children, stating it is more than just an arts program. It's a lifeline for children to thrive in creative, supportive environments where they can express themselves freely, build confidence and develop essential life skills. Also mentioned was a class performance to take place at El Dorado High School on June 29th. Calvin Hutchinson spoke about how much he loves to have fun at No Limits Creative Arts.

Chair Perez closed the Public Hearing at 6:51 p.m.

Motion by Rocke second by Evans and carried on a (6-0-0-0) vote to approve Use Permit No. 2024-09 as recommended by Staff

Ayes: Evans, Guerrero, Ingalls, Navarro, Rocke, Perez
Noes: None
Abstain: None
Absent: None

2. Applicant: Glen Gwatney of Anacal Engineering, representing the property owner Greenleaf LLC

Project Location: 975 S. Lakeview Avenue (Tentative Parcel Map No. TPM 2024-143)

TENTATIVE PARCEL MAP NO. TPM 2024-143. A REQUEST TO SUBDIVIDE A 2.66-ACRE PARCEL AT 975 S. LAKEVIEW AVENUE (APN 346-341-29) INTO TWO PARCELS OF 1.38 ACRES (PARCEL 1) AND 1.27 ACRES (PARCEL 2) WITHIN THE C-M (COMMERCIAL MANUFACTURING) ZONING DISTRICT

Recommended Action: Adopt Resolution approving the project as recommended by Staff.

Chair Perez opened the Public Hearing at 6:53 p.m.

Director Lambert introduced Planning Manager Andrew Gonzales to present the Staff Report. Mr. Gonzales presented the request to reconfigure existing property lines for operational efficiency. The parcel will be divided into two parcels: one at 1.38 acres and one at 1.27 acres.

Vice Chair Rocke and Commissioner Ingalls asked questions about implications related to the Parcel Map that may result in non-conforming parking and may result in additional non-conforming aspects on the subject site. Planning Manager Gonzales clarified that the project aligns with current business operations and meets parking requirements.

Applicants Leah and Peter Monge of Greenleaf LLC, and residents of Santa Ana, were present to explain more fully the project and the need to divide the parcels in a specific configuration.

Chair Perez closed the Public Hearing at 7:04 p.m.

Motion by Evans second by Guerrero and carried on a (6-0-0-0) vote to approve Tentative Parcel Map No. TPM 2024-143 as recommended by Staff

Ayes: Evans, Guerrero, Ingalls, Navarro, Rocke, Perez
Noes: None
Abstain: None
Absent: None

REGULAR AGENDA:

1. Applicant: City of Placentia

Project Location: Citywide

Study Session Regarding Residential Development Standards

Recommended Actions: It is recommended that the Planning Commission take the following actions:

1. Discuss the Residential Development Standards Survey Data
2. Provide staff direction as appropriate

Director Lambert introduced and presented the second Study Session regarding the Residential Development Standards survey. The survey includes categories such as single-family residential parking, garage space dimensions, and outdoor driveway space dimensions.

Consensus on Certain Residential Standards

- **The Planning Commission agreed to maintain 20' by 20' garage dimensions and 10' by 20' outdoor driveway space dimensions, per parking space for single-family and multiple-family development.**
- Discussion continued regarding the need for additional space for ADA-compliant parking and maneuvering areas.

Backup Distance for Guest Parking

- Director Lambert presented the required backup distance for guest parking in multiple-family residential zones.
- Staff's recommendation is to leave the current 20' minimum or consider increasing it to 24'.
- **The Planning Commission agreed to increase the backup distance to 24' for both uncovered and covered parking in multiple-family zones.**

Discussion on Private Street Widths and Fire Code Minimums

- Director Lambert explained the current use of the fire code to determine minimum private street widths, mentioning specific zones.
- Chair Perez suggested a 20' minimum.
- Commissioner Navarro raised concerns about trash bin placement and the impact on street widths, suggesting a 26' minimum.
- Commissioner Ingalls supported the current 20' fire minimum, noting that it aligns with fire code requirements for multi-family residences over four stories.
- **The Planning Commission agrees to maintain the 20' minimum for internal private streets.**

Developer Feedback and Potential Changes

- Director Lambert mentioned that the development community generally supports the current minimums, with no significant pushback observed.
- Vice Chair Rocke suggests tabling the discussion to gather more information, including fire truck dimensions.
- Commissioner Ingalls emphasizes the importance of considering worst-case scenarios, such as aerial ladder operations for four-story buildings.
- **Director Lambert proposes renaming "private streets" to "internal access ways" to avoid official connotations.**

Internal Access Ways and Clear Drivable Areas

- Director Lambert discussed the need to differentiate between internal access ways with and without street parking, focusing on clear drivable areas.
- Commissioner Navarro supported the current minimum drive aisle widths but suggested the need for exhibits to clarify the dimensions.
- Chair Perez highlighted the importance of considering county Fire Department standards, which may supersede city standards.
- **Director Lambert agreed to provide more data and schematics for internal accessways, including different scenarios with garages and parallel parking.**

Driveway Approach Widths and Fence Regulations

- Director Lambert outlined the current driveway approach widths in Placentia, a 20' minimum and 24' maximum.
- Commissioner Navarro supported the proposed driveway approach widths, noting the need for clearance for back-to-back garages.
- Chair Perez and Director Lambert discussed the constraints of driveway approaches, ensuring they do not exceed beyond property lines.

Single-Family and Multi-Family Fence and Gate Regulations

- Director Lambert explained the current fence regulations, including height limits, the need for building permits for certain fences as well as the need for see-through fences when over 3' tall in front yards.
- Commissioner Ingalls suggested a 20' setback for driveway gates to avoid blocking public rights-of-way.

- **Regarding Single-Family Fencing, Planning Commission agreed that there would be no changes.**
- Commissioner Navarro proposed a 20' minimum setback for multi-family driveway gates, with the possibility of increasing it based on traffic conditions.
- **The Commission agreed to leave the final decision on fence setback dimensions to the City Engineer or Traffic Engineer.**

Turnaround Spaces for Trash and Delivery Vehicles

- Commissioner Navarro suggested adding language to require turnaround spaces for vehicles to exit properties in a forward direction.
- Vice Chair Rocke supported the idea but wants to review it in the next meeting to ensure it is practical and effective.
- Commissioner Ingalls mentioned that commercial projects often coordinate with trash providers to ensure adequate turnaround spaces.
- **The Commission agreed to add the turnaround space requirement to the discussion for further review.**

Final Remarks and Meeting Adjournment

- Director Lambert expressed gratitude for the Commission's input and acknowledges the need for additional meetings to explore potential changes to standards.
- **Vice Chair Rocke suggested taking time to prepare the necessary documents and diagrams for the next meeting.**
- Commissioner Navarro and Chair Perez thanked Director Lambert and his team for their hard work and expressed satisfaction with the progress made.
- The meeting was adjourned with a plan to reconvene on April 8, or at the next regular meeting for further discussion and achieve consensus regarding the proposed changes.

OLD BUSINESS: None

NEW BUSINESS: None

DEVELOPMENT REPORT: None

DIRECTOR'S REPORT: None

PLANNING COMMISSION REQUESTS:

Commissioner Navarro and Chair Perez thanked Staff for their hard work on Residential Standards.

ADJOURNMENT:

Chair Perez adjourned the Regular Meeting of the Planning Commission at 8:45 p.m. to the regular meeting of Tuesday, April 8, 2025, at 6:30 p.m. in the City Council Chambers at 401 East Chapman Avenue, Placentia, CA.

Submitted by,

Joseph M. Lambert,
Secretary to the Planning Commission



Placentia Planning Commission

AGENDA STAFF REPORT

TO: PLANNING COMMISSION
FROM: DIRECTOR OF DEVELOPMENT SERVICES
DATE: MAY 13, 2025
SUBJECT: **GENERAL PLAN CONFORMANCE FINDING FOR FISCAL YEAR 2025-26 CAPITAL IMPROVEMENT PROGRAM**

RECOMMENDATION:

It is recommended that the Planning Commission adopt Resolution No. PC-2025-04, A Resolution of the Planning Commission of the City of Placentia, California, finding that the Fiscal Year 2025-26 Capital Improvement Program conforms to the adopted General Plan and recommending the City Council approve the Capital Improvement Program.

DISCUSSION:

California Government Code § 65401 requires the City to prepare a coordinated program (i.e., Capital Improvement Program (CIP)) identifying projects recommended for planning, initiation or construction during the ensuing fiscal year. In accordance with state law, the Planning Commission must review and report on the conformity of the CIP with the adopted General Plan.

The CIP serves as a single comprehensive plan of proposed capital improvement projects for the budget year FY 2025-26. The CIP is a budgeting and strategic planning tool used to determine the extent of future needs and sources of funding. The CIP also serves as a guide for the efficient evaluation, prioritization and implementation of public improvements and facilities. For FY 2025-26, the City has identified 20 new projects totaling approximately \$12.8 million which it expects to begin work on during the forthcoming fiscal year.

As part of the CIP process, capital improvement projects are evaluated to determine the estimated cost and prioritized to take full advantage of Federal, State, Orange County and other funding from outside agencies. The CIP is reviewed yearly, during which time the City's needs may be re-prioritized and the City's financial status may be re-evaluated.

The first CIP priority is safety within the City's right-of-way, parks, and facilities. This priority is addressed throughout each section of the CIP document in numerous ways, including streets, traffic control and operations, parks, public buildings, bridges, sewer systems and storm drain systems. Additionally, a primary focus of the CIP is to preserve and reinvest in the City's physical assets, as reflected in each of the ten CIP sections. This includes pavement rehabilitation projects for residential and arterial streets as well as

improvements at City facilities and ADA improvements to improve accessibility. The proposed CIP recommends several projects which address facilities or equipment that are no longer adequate to meet demand or serve our citizens. These projects include the replacement or upgrade of facilities or equipment, expansion or renovation of an existing facility, or construction of a new facility. Another important component of the CIP is to implement projects identified within a master plan or other planning document that are needed to maintain or meet adequate levels of service. These projects may include new facilities or improvements toward existing facilities. Finally, the CIP addresses several future planned projects through funding of studies or project construction plans.

Conformity Findings

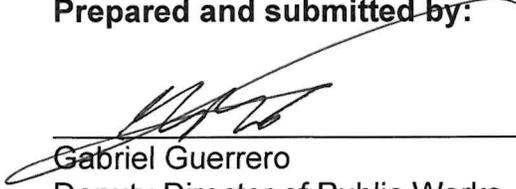
The basis for making a conformity finding is determining that each project fits into a goal or implementation policy as outlined in the adopted General Plan. For example, if a project is proposed to pave over several acres of open space, and that open space is called out in the Open Space Element as being protected, then the project does not conform with that General Plan Element. Another example is, if a proposed project resulted in a widened street or added bicycle or pedestrian paths to increase mobility, such a project would conform with the Mobility Element goals to increase accessibility and mobility for alternative modes of transportation.

City Staff has reviewed each of the proposed projects to be undertaken during Fiscal Year 2025-26 in relation to the adopted General Plan. The individual elements of the General Plan have goals, policies or implementation programs to compare the projects against the narrative of the adopted elements and make a determination that they are each in conformity with the adopted General Plan for the City of Placentia. Additionally, the Commission's finding that a proposed CIP item conforms to the General Plan does not necessarily mean the City will have the ability to implement or construct a certain CIP item. All CIP projects and plans must go through separate individual environmental and Council review. Accordingly, Staff is recommending that the Planning Commission find that the proposed CIP projects are in conformity with the adopted General Plan.

Environmental Review

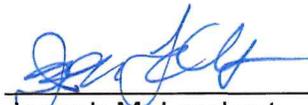
The proposed Finding of General Plan Conformity is not a project as defined by the California Environmental Quality Act (CEQA). The finding of conformity does not commit the City to any of the identified projects in the CIP. Therefore, approval of the CIP qualifies for an exemption from the California Environmental Quality Act (CEQA) per Guideline 15061 (b)(3) for activities that have no potential for causing a significant effect on the environment. Furthermore, individual projects identified within the CIP will be reviewed in accordance with CEQA requirements prior to their implementation and when specific project details are available for analysis.

Prepared and submitted by:



Gabriel Guerrero
Deputy Director of Public Works

Reviewed and approved by:



Joseph M. Lambert
Director of Development Services

Attachments:

1. Resolution No. PC-2025-04
2. General Plan Conformity Matrix

RESOLUTION NO. PC-2025-04

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PLACENTIA, CALIFORNIA, FINDING THAT THE FISCAL YEAR 2025-26 CAPITAL IMPROVEMENT PROGRAM IS EXEMPT FROM THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) PURSUANT TO CEQA GUIDELINES SECTION 15061 (B)(3), AND FINDING THAT THE FISCAL YEAR 2025-26 CAPITAL IMPROVEMENT PROGRAM CONFORMS TO THE ADOPTED GENERAL PLAN AND RECOMMENDING THE CITY COUNCIL APPROVE THE CAPITAL IMPROVEMENT PROGRAM

A. Recitals.

(i). On May 13, 2025, this Planning Commission considered the conformity of the proposed Fiscal Year 2025-26 Capital Improvement Program to the adopted General Plan.

(ii). This Planning Commission has reviewed and considered all elements of the proposed Capital Improvement Program, including written staff reports and verbal testimony presented.

(iii). All legal prerequisites to the adoption of this Resolution have occurred.

B. Resolution.

NOW, THEREFORE, the Planning Commission of the City of Placentia does hereby find, determine, and resolve as follows:

1. This Commission hereby specifically finds that all the facts as set forth in the Recitals, Part A., of this Resolution are true and correct.

2. This Commission finds, and recommends the City Council find, that the Fiscal Year 2025-26 Capital Improvement Program (Attachment 2 to the Staff Report dated May 13, 2025, and incorporated by reference herein) is in conformance with the Placentia General Plan.

3. The proposed Fiscal Year 2025-26 Capital Improvement Program promotes implementation of the goals and policies set forth in the adopted General Plan.

4. The Planning Commission hereby makes, and recommends the City Council make, the following finding: There is no possibility that the proposed Fiscal Year 2025-26 Capital Improvement Program may have a significant effect on the environment and said amendments are therefore not subject to the requirements of the California Environmental Quality Act ("CEQA") pursuant to the provisions of

§15061(b)(3) of Division 6 of Title 14 of the California Code of Regulations, and the City of Placentia Environmental Guidelines.

5. The Planning Commission hereby recommends that, upon adoption of the Fiscal Year 2025-26 Capital Improvement Program, a Notice of Exemption in accordance with CEQA be filed with the Orange County Clerk-Recorder, as required by law.

6. This Commission finds, and recommends the City Council find, that the facts supporting the above specified findings are contained in the staff report and exhibits, and information provided to this Planning Commission with respect to the proposed Fiscal Year 2025-26 Capital Improvement Program.

7. This Commission hereby recommends that the City Council of the City of Placentia approve the proposed Fiscal Year 2025-26 Capital Improvement Program.

8. The Secretary of this Commission shall:

a. Certify to the adoption of this Resolution; and

b. Forthwith transmit a certified copy of this Resolution to the City Council of the City of Placentia together with all documents prepared with respect to these considerations and transcripts of any and all hearings conducted with respect to the action recommended for approval herein.

ADOPTED, AND APPROVED this 13th day of May 2025

FRANK PEREZ, CHAIR

I, Joseph M. Lambert, Secretary to the Planning Commission of the City of Placentia, do hereby certify that the foregoing Resolution was introduced at a regular meeting of the Planning Commission of the City of Placentia held on the 13th day of May 2025, and was passed at this regular meeting of the Planning Commission of the City of Placentia held on the 13th day of May, 2025, by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

ATTEST:

JOSEPH M. LAMBERT,
SECRETARY TO THE PLANNING COMMISSION

APPROVED AS TO FORM

CITY ATTORNEY

**City of Placentia - General Plan Conformity
Proposed FY 2025-26 Capital Improvement Projects**

| Project Name | General Plan Element |
|---|-----------------------------|
| UST Removal and Replacement at Corporate Yard | Safety |
| Old Town Streetscape Improvements | Mobility |
| Powell Building Improvement Project - Window Replacements | Open Space & Recreation |
| Whitten Center Kitchen Renovation | Open Space & Recreation |
| Aguirre Building Renovation | Open Space & Recreation |
| Tynes Gym Roof Replacement | Open Space & Recreation |
| Samp and Los Vaqueros Park Playground Renovations | Open Space & Recreation |
| Replace Park Benches | Open Space & Recreation |
| Whitten Center Pool Deck Repair | Open Space & Recreation |
| Los Vaqueros Storm Drain | Open Space & Recreation |
| Old Town Streetscape Improvements | Mobility |
| FY 25-26 Annual Sidewalk and Curb Ramp Repairs | Mobility |
| Golden Avenue Bridge Replacement Project Construction | Mobility |
| FY 24-25 Residential Street Rehabilitation | Mobility |
| Desktop Computer Replacement Program | Sustainability |
| Enterprise Camera and Security Upgrades | Safety |
| Public Safety Operations HW Upgrades | Safety |
| City Facilities Server/Network Upgrades | Sustainability |
| Citywide Signal Hardware Upgrades | Safety |
| Fleet Lift Equipment | Sustainability |
| Fire Department Equipment | Safety |



City of Placentia

Development Services Department

MEMORANDUM

TO: PLANNING COMMISSION

FROM: DEVELOPMENT SERVICES DEPARTMENT

VIA: JOE LAMBERT, DIRECTOR OF DEVELOPMENT SERVICES

DATE: MAY 13, 2025

**SUBJECT: STUDY SESSION TO REVIEW CHAPMAN CORRIDOR
REVITALIZATION PLAN PROGRESS**

BACKGROUND:

On October 1, 2019, City Council adopted the comprehensive General Plan Update. This update included all Elements of the General Plan except the Housing Element, which was adopted as part of the required 6th Cycle Regional Housing Needs Assessment (RHNA) allocations in March 2022, which is in the process of State certification. The Land Use Element of the 2019 General Plan strongly encourages enhancements and improvements to the visual image, physical design characteristics, economic vitality and infrastructure of the Chapman Corridor.

City Council previously authorized Placeworks, Inc. (Placeworks) to complete the Chapman Corridor Revitalization Plan (CCRP) document and supporting environmental documentation furthering the goals of the General Plan. On February 7, 2023, City Council conducted the first Study Session related to the Chapman Corridor Plan and the second Study Session occurred on October 3, 2023. City Council provided direction on several key policies to move the effort forward. On March 18, 2025, City Staff and Placeworks presented the draft Chapman Corridor Revitalization Plan with draft Development Standards and a draft associated Streetscape Master Plan. This was the final Study Session to receive policy direction prior to moving forward with the final approval process.

DISCUSSION:

The Chapman Corridor Revitalization Plan development standards and regulations will foster responsible, progressive and attractive development to meet City Council's goals for the corridor. Medium density, mixed-use, and new commercial development will bring new businesses and residents to the Chapman Corridor, and to the adjacent Old Town Placentia and TOD Districts, benefiting the traditional downtown with new shoppers and diners.

Another benefit of the Revitalization Plan is to plan for appropriate successor commercial and mixed-use development that will unify the Chapman Corridor from Placentia Avenue to Kraemer Boulevard. Staff envisions that the plan will encourage the consolidation of smaller

parcels for new development, providing a catalyst for development, thereby increasing pedestrian activity and revenue to the City in the form of property taxes, sales tax, Transient Occupancy tax and impact fees. This will also result in a more unified aesthetic for the Chapman Corridor, which currently appears aesthetically and visually inconsistent between Placentia Avenue and Kraemer Boulevard.

DISCUSSION:

This agenda item shall serve as a forum for the Planning Commission to ask any questions of staff regarding the Draft Chapman Corridor Revitalization Plan and associated documents. It is anticipated that this item will come back to the Planning Commission as a Public Hearing when the Chapman Corridor Revitalization Plan is ready for adoption and codification into the Placentia Municipal Code.

Recommended Action: It is recommended that the Planning Commission receive and file the attached City Council Staff Report and staff presentation and ask any questions of staff as appropriate.

Attachments:

1. City Council Staff Report dated March 18, 2025
2. Attachment 1 to City Council Report - CCRP PowerPoint
3. Attachment 2 to City Council Report - Draft CCRP



Agenda Item No: 3.a

PLACENTIA CITY COUNCIL AGENDA REPORT

Meeting Date: March 18, 2025

Submitted by: Joe Lambert

From: Development Services

Subject:

Third Study Session to Review Chapman Corridor Revitalization Plan Progress

Financial Impact:

Fiscal Impact:

No fiscal impacts are associated with this Study Session. Potential significant economic benefit to the local Placentia economy, including jobs, public & private investment, and annual/one-time revenue.

Summary:

On October 1, 2019, City Council adopted the comprehensive General Plan Update. This update included all Elements of the General Plan except the Housing Element, which was adopted as part of the required 6th Cycle Regional Housing Needs Assessment (RHNA) allocations in March 2022, which is in the process of State certification. The Land Use Element of the 2019 General Plan strongly encourages enhancements and improvements to the visual image, physical design characteristics, economic vitality and infrastructure of the Chapman Corridor. City Council previously authorized Placeworks, Inc. (Placeworks) to complete the Chapman Corridor Revitalization Plan document and supporting environmental documentation furthering the goals of the General Plan. On February 7, 2023, City Council conducted the first Study Session related to the Chapman Corridor Plan and the second Study Session occurred on October 3, 2023. City Council provided direction on several key policies to move the effort forward. At this time, City Staff and Placeworks will present the draft Chapman Corridor Revitalization Plan with draft Development Standards and will also show progress made regarding a draft associated Streetscape Master Plan. Staff intends this to be the final Study Session to receive policy direction prior to moving forward with the final approval process.

Recommendation:

Recommended Action: It is recommended that the City Council:

1. Receive and file the Staff Report and presentation; and
2. Solicit public comments (if any) regarding the draft Chapman Corridor Revitalization Plan and supporting documentation; and
3. Provide direction to Staff as appropriate regarding the proposed Chapman Corridor Revitalization Plan, Draft Development Standards and associated Streetscape Master Plan.

Strategic Plan Statement:

This item is consistent with the City Council approved 5-Year Strategic Goal to:

Promote Community and Economic Development, as Objective Number 3.5 is to Initiate the Chapman Corridor Specific Plan and Objective Number 3.6 is to create design review guidelines and procedures. This agenda item will specifically result in creation of the Chapman Corridor Specific Plan (or equal Zoning document) and will also create design review guidelines specific to the Chapman Corridor.

Discussion:**BACKGROUND:**

The General Plan is the City's land use constitution, a legal policy document that is mandated to exist by the State of California. The General Plan is intended to reflect the community's values and set policies to guide in the development, maintenance, and use of the City's built environment. It forms the basis for future decision making as it relates to the mandatory and optional Elements of the General Plan. The Land Use Element is one of seven mandatory General Plan Elements required by the State of California. On October 1, 2019, City Council adopted the comprehensive General Plan Update, which included an updated Land Use Element. The Land Use Element of the 2019 General Plan strongly encourages enhancements and improvements to the visual image, physical design characteristics, economic vitality and infrastructure of the Chapman Corridor and other major corridors as indicated in the subsequent paragraphs.

- Goal LU-1 states: Provide a well-balanced land use pattern that accommodates existing and future needs for housing, commercial, industrial and open space/recreation uses, while providing adequate community services to City residents. Policy LU-1.10 further states: Create specific zoning or plans for major corridors within the City. This would include the Chapman Avenue corridor and the Placentia Avenue corridor, among other major thoroughfares.
- Goal LU-3 states: Revitalize underutilized, abandoned or dilapidated commercial, industrial and residential uses and properties. Policy LU-3.1 furthers this goal by stating: Encourage opportunities for redevelopment and improvements in the Old Town area, the TOD district, industrial areas, neighborhoods in the southern sector of the City, and commercial centers along major roadway corridors.
- Goal LU-5 states: Improve urban design in Placentia to ensure that development is both architecturally attractive and functionally compatible and to create identifiable neighborhoods, and community areas. Policy LU-6.6 furthers this goal by stating: Improve urban design in Placentia to ensure that development is both architecturally attractive and functionally compatible and to create identifiable neighborhoods, and community areas.
- Goal LU-6 states: Enhance and improve the visual image, economic vitality and infrastructure of the Old Town area, TOD, and surrounding areas, like the future Chapman Corridor. Policy LU-6.6 furthers this goal by stating: Focus planning and economic development efforts to spur development and infrastructure improvement on major transportation corridors, such as the future Chapman Avenue corridor.
- Goal LU-10 states: Create enhanced connectivity with California State University

Fullerton (CSUF) campus community. Policy LU-10.2 furthers this goal by stating: In creating the aforementioned corridor plans, the City shall take into consideration the nearby Cal State University Fullerton campus community and capitalize on its proximity.

In October 2022, City Council approved a Professional Services Agreement with Placeworks to draft the Chapman Corridor Revitalization Plan document and supporting environmental documentation furthering the aforementioned goals of the General Plan. Subsequent or concurrent with approval of the corridor plan document, staff anticipates the adoption of a Chapman Corridor Streetscape Master Plan.

On February 7, 2023, City Council conducted the first Study Session related to the Chapman Corridor Plan and provided direction on several key policies to move the effort forward. The first set of policy questions were related to the corridor boundaries. The second set of policy questions were related to land use and site testing, including: 1. What does Council envision for the future of Chapman Corridor? 2. What are key objectives for Chapman Corridor? and 3. What mobility options should be considered for Chapman Avenue and connections to Old Town and the future Metrolink Station? The third set of policy questions were related to additional uses and mobility (transportation). Those questions were: 1. What additional uses and or types of development should be explored? and 2. Which mobility options and connections should this plan prioritize?

On October 3, 2023, City Council conducted the second Study Session related to the Chapman Corridor Plan and provided direction on additional key policies to move the effort forward. Those policy questions were as follows:

- Is 3 to 4 stories (and possibly 5 stories in some locations) appropriate for the Chapman Corridor? Should certain land uses be considered or more strongly encouraged? Is there concurrence with Staff regarding the land use districts and sub-areas? Is there concurrence with Staff regarding the omission of street parking? At that meeting, City Council concurred with the general direction of the draft land use plan and concurred with eliminating street parking on one remaining section of eastbound Chapman Avenue. After receiving Council input, Placeworks and staff moved forward in drafting the Chapman Corridor Specific Plan with development and design standards.

DISCUSSION:

The Chapman Corridor Revitalization Plan development standards and regulations will foster responsible, progressive and attractive development to meet City Council's goals for the corridor. Medium density, mixed-use, and new commercial development will bring new businesses and residents to the Chapman Corridor, and to the adjacent Old Town Placentia and TOD Districts, benefiting the traditional downtown with new shoppers and diners.

Another benefit of the Revitalization Plan is to plan for appropriate successor commercial and mixed-use development that will unify the Chapman Corridor from Placentia Avenue to Kraemer Boulevard. Staff envisions that the plan will encourage the consolidation of smaller parcels for new development, providing a catalyst for development, thereby increasing pedestrian activity and revenue to the City in the form of property taxes, sales tax, Transient Occupancy tax and impact fees. This will also result in a more unified aesthetic for the Chapman Corridor, which currently appears aesthetically and visually inconsistent between Placentia Avenue and Kraemer Boulevard.

Placeworks (with Staff oversight) prepared a presentation (Attachment 1) for City Council review. The presentation will summarize a review of the project to date, summary of public outreach, proposed land use districts and sub-areas and existing conditions (including land use, urban design, mobility, opportunity areas, mobility considerations, and policy guidance/questions, with next steps. The presentation will also show street sections prepared by consultants to demonstrate potential Chapman Avenue street sections, and elements from the draft Chapman Corridor Streetscape Master Plan. The draft Chapman Corridor Revitalization Plan (CCRP) is also attached (Attachment 2) for review. The goal of this Study Session is to receive final City Council input and public input prior to moving forward with the approval process.

At this time, City Staff and Placeworks will present the progress made since February and will present additional policy questions to City Council.

Next Steps

Spring 2025:

- Finalize the draft Chapman Corridor Specific Plan document, with final Economic Benefit Analysis.
- Ongoing Community Outreach, including but not limited to utilization of website updates.
- Initialize approval of Corridor Revitalization Plan, with any necessary CEQA studies (final Plan adoption with CEQA certification)

Summer 2025:

- City Council approval of Corridor Revitalization Plan, with adoption of Chapman Corridor Streetscape Master Plan.

Fiscal Impact Summary:

Fiscal Impact:

No fiscal impacts are associated with this Study Session. Potential significant economic benefit to the local Placentia economy, including jobs, public & private investment, and annual/one-time revenue.

Attachments

1. [DRAFT CCRP_PPT.pdf](#)
2. [DRAFT_CCRP.pdf](#)

Chapman Corridor Revitalization Plan

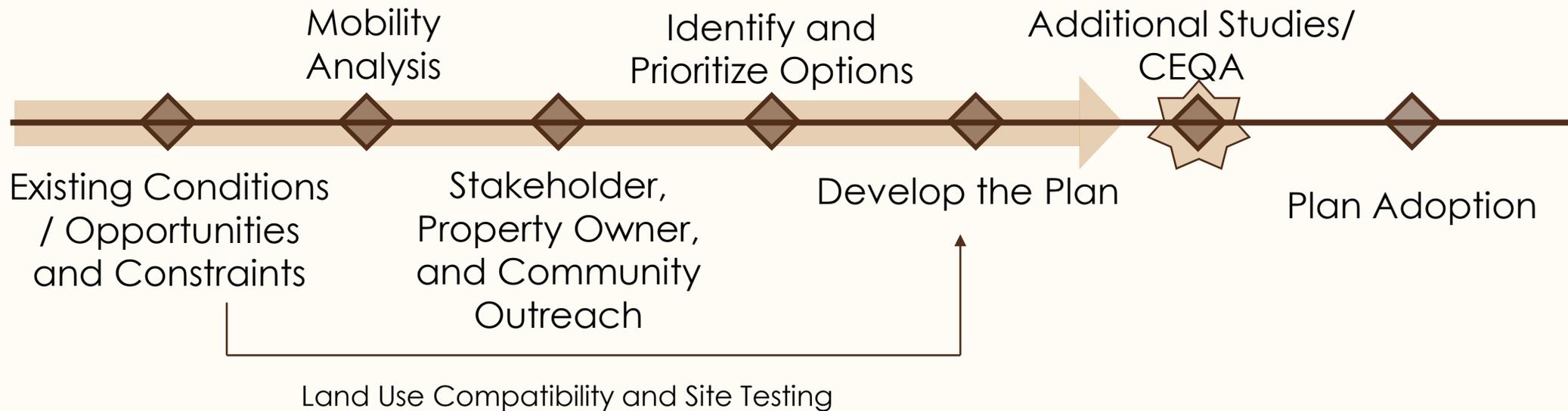
City Council Study Session
3/18/2025

Agenda

- Update Council on the Chapman Corridor Revitalization Plan
- Provide an overview of the draft land use plan development regulations and proposed streetscape improvements

Project Overview

- Kicked off in 2022 to explore options for entitlement and incentives for revitalization along the corridor
- Adoption of this plan will result in rezoning of the Chapman Corridor area.



Nearby Planning Efforts

- Old Town Placentia Revitalization Plan (2017)
- Packing House TOD (2017)
- Packing Housing TOD Expansion (2024)
- Update to SP-5 (underway)



Outreach Recap

- In-person public workshop
- On-line survey
- City Council Study Sessions (2)
- Summer Concerts
- Oldtown Placentia Meetings
- C.A. Weekly Articles
- City Website Spotlight
- Social Media Post



Q5: What type of land uses would you like to see? (Multiple Choice)



Restaurants / Cafes

65 respondents voted for this option



Small Scale Retail

46 respondents voted for this option



Dynamic Mixed-Use Civic Center

45 respondents voted for this option



Mixed-Use Residential

34 respondents voted for this option

Vision & Guiding Principles

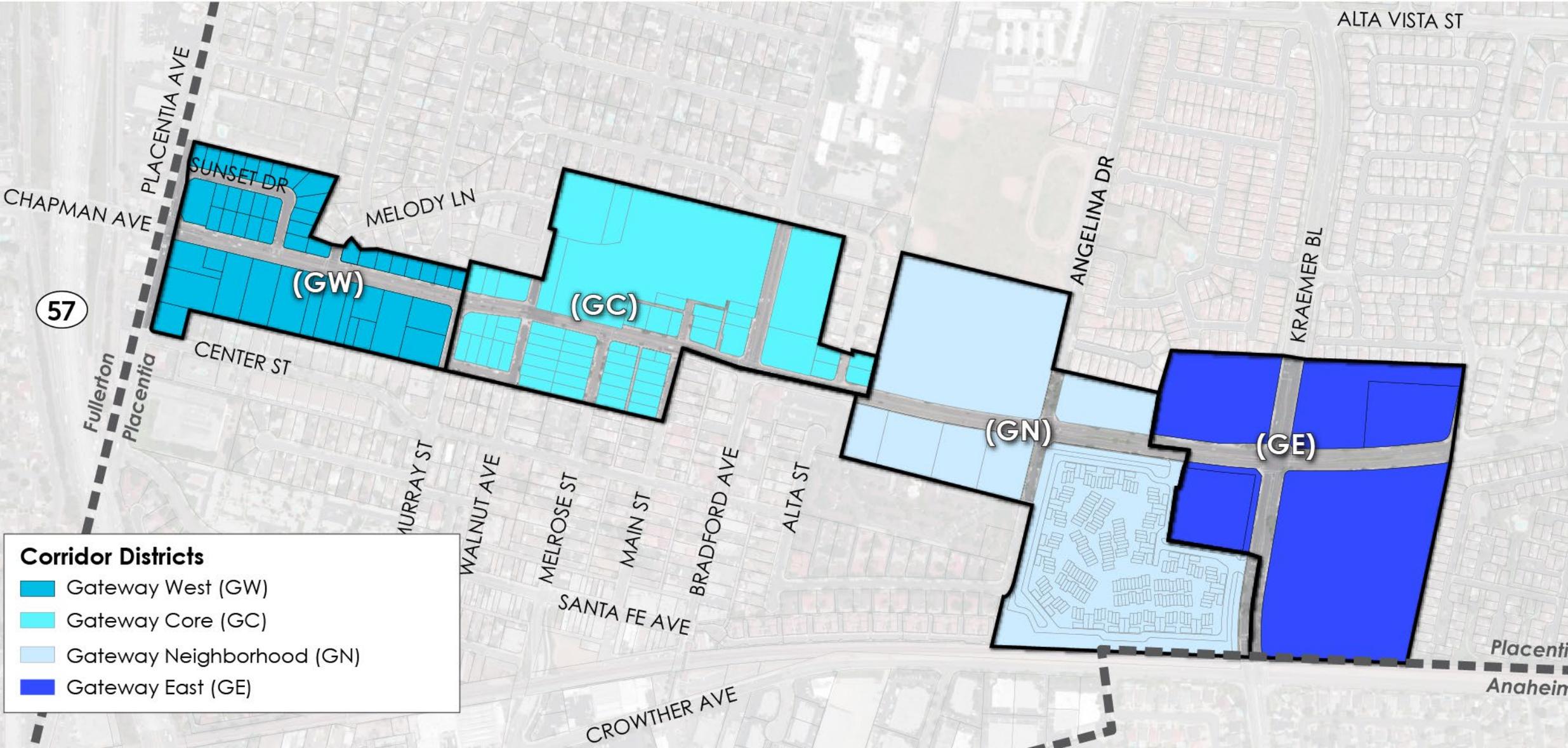
The Chapman Corridor is a prominent **gateway** into the city, Old Town, and the Packing House District. The street is a vibrant area where the community comes for civic activities at Kraemer Park, the library, and City Hall. **Pride of ownership** shows throughout the corridor where property owners have upgraded and maintained their shop fronts, homes, and community entries. **New mixed-use** developments have increased the city's housing stock and provided cafes and other businesses for residents and visitors. Chapman Avenue itself provides **an efficient way to connect** to the freeway, Old Town, and the Metrolink Station **by driving, taking transit, cycling, or walking**. A hotel along the corridor provides an **additional lodging** option for visitors to Placentia and nearby Cal State Fullerton. Residents who used to drive straight through the area now make a point to stop at their **favorite local spot** on their way to or from their next destination. This revitalized area anchors the western boundary of the City, signaling to all that they have entered Placentia—"a pleasant place."

Vision & Guiding Principles

- Create subareas to tailor land use standards.
- Create new flexible land use districts allowing for new housing units and mixed-use development.
- Preserve the function of Chapman Avenue but add safe multimodal options.
- Revitalize frontage along Kraemer Park and continue to support civic uses along the corridor.
- Link to Old Town District and Packing House District (TOD) areas of town.
- Allow for new and expanded retail and commercial opportunities.
- Streetscape beautification along Chapman Avenue.



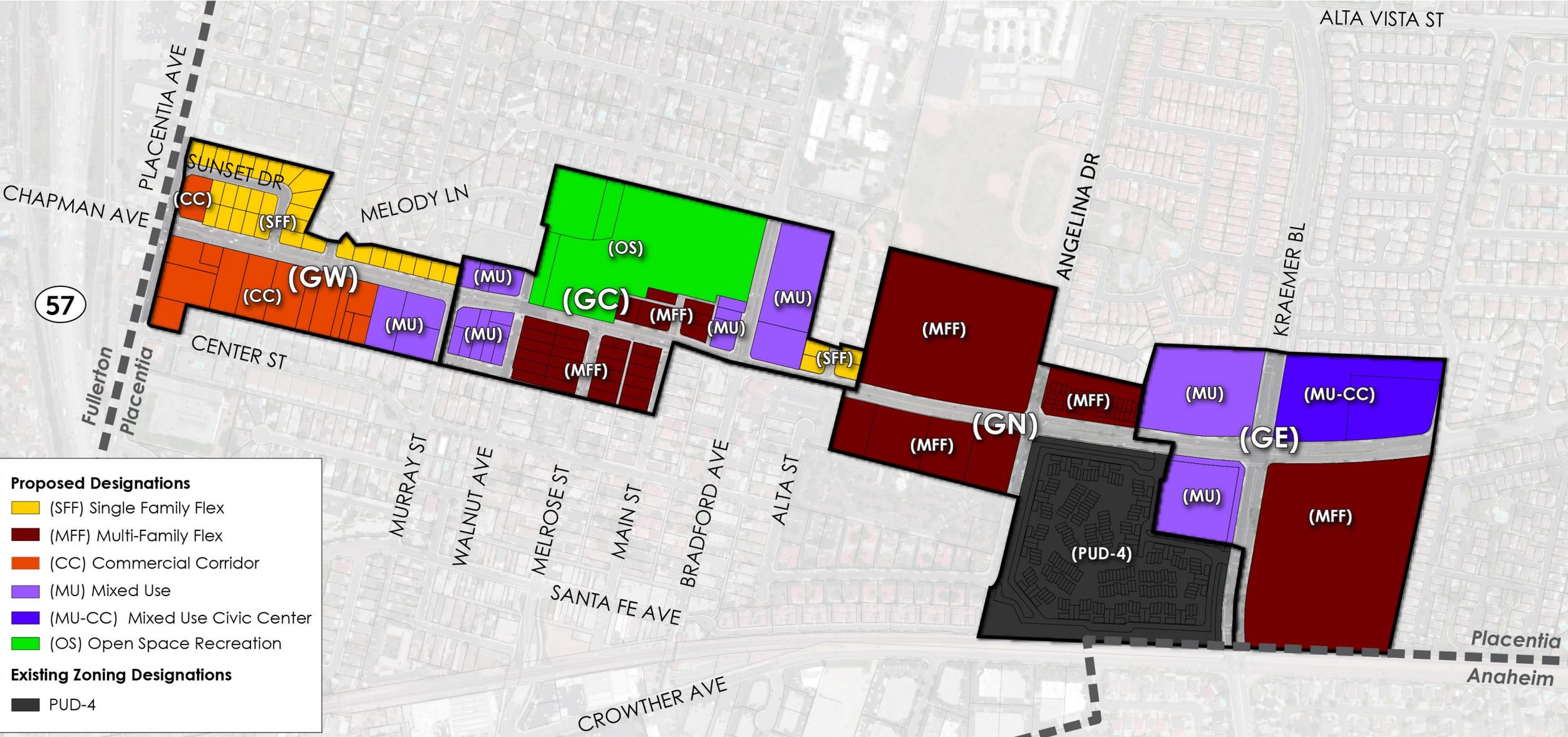
Corridor Districts



Corridor Districts

- Gateway West (GW)
- Gateway Core (GC)
- Gateway Neighborhood (GN)
- Gateway East (GE)

Proposed Land Use



Proposed Designations

| Designation | Description | Example of Permitted Uses |
|--------------------------|--|--|
| Single Family Flex (SFF) | Allows existing SF to continue Allows adaptive reuse of structures for flexibility of commercial (i.e. offices in older homes as found in Orange and Fullerton). | Existing single-family residential, home occupation, retail less than 5,000 sf, Office (must occupy existing building), Public Park, Playground or Plaza |
| Multi Family Flex (MFF) | Would allow for multi-family development between 32 and 45 dwelling units per acre depending on the sub area. | Multi-family Residential |
| Commercial Corridor (CC) | Allows current commercial uses to continue to operate. This designation preserves commercial uses but increases intensity to incentivize further redevelopment. | General Retail, Grocery Stores, neighborhood market, Restaurants, Cafes, Offices, Public Park, Playground or Plaza |
| Mixed-Use* (MU) | Accommodates future catalyst Site (Angelina Development). Allows for height 2-3 stories and 3-4 stories, 5 if certain conditions are met, varies by sub area. Would also allow for a hotel in the Gateway West sub area. | Multi-family Residential, General Retail less than 5,000 SF, Neighborhood Market, Restaurants, Cafes, Offices, Public Park, Playground or Plaza |
| Civic Center Mixed-Use* | Provides flexibility in uses and standards for future use of the Civic Center. Allow for height 3-4 stories, 5 if certain conditions are met. | City Hall and related uses, Multi-family Residential, General Retail, Neighborhood Market, Restaurants, Cafes, Offices, Public Park, Playground or Plaza |

*All Mixed-Use would allow for housing but would not require ground floor commercial, configurations could be vertical or horizontal.

Single Family Flex

Existing Single Family



Existing Single Family



Home Business Conversion Example



Home Business Conversion Example



Multi-Family Flex

Potential Multi-Family Residential Examples



Commercial Corridor

Potential Hotel



Potential Hotel



Existing Commercial



Potential Commercial / Retail Example



Mixed-Use Catalytic Project

Angelina Development – Kraemer Land Company



ANGELINA
PLACENTIA | CALIFORNIA
KRAEMER LAND COMPANY | 19-128
DATE 3 | 3 | 21



VIEW 041 EAST CHARMAN AVE. AFTER 1 | A9.1



ANGELINA
PLACENTIA | CALIFORNIA
KRAEMER LAND COMPANY | 19-128
DATE 3 | 3 | 21



VIEW ON SOUTH KRAEMER BLVD. AFTER | A9.1



ANGELINA
PLACENTIA | CALIFORNIA
KRAEMER LAND COMPANY | 19-128
DATE 3 | 3 | 21



COVER SHEET | CS 1

Mixed-Use

Mixed-Use Examples



Commercial / Retail



Multi-Family Residential



*All Mixed-Use would allow for housing but would not require ground floor commercial, configurations could be vertical or horizontal.

Civic Center Mixed Use



La Habra Civic Center – Land Swap



Laguna Niguel “City Center”



Figure 2-4: Conceptual Land Use Plan, Ground Level



Figure 2-5: Conceptual Land Use Plan, Second Floor

Norwalk Entertainment Center – Developer Led Effort

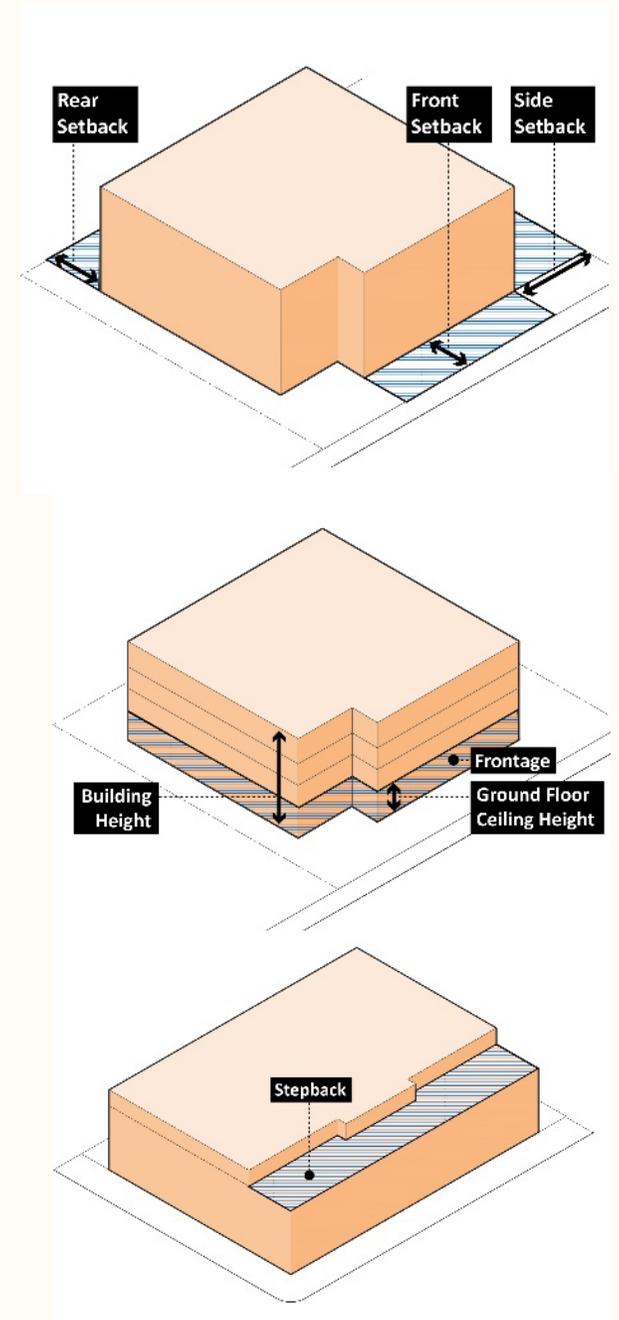
Land Use: Proposed Buildout

| Designation | Acres | Max Density (DU/AC) | Max FAR | Existing Residential Units | Existing Sq Ft | Target Residential Units | Target Non-residential Sq Ft | Change |
|--|-------|---------------------|----------|----------------------------|----------------|--------------------------|------------------------------|------------------------------|
| Single Family Flex (SFF) | 5.7 | 6 | 0.35 | 36 | -- | 34 | -- | (2) units |
| Mixed-Use (MU) & Mixed-Use Civic Center (MU-CI) ¹ | 21.7 | 35–45 | 0.35-1.0 | 16 | 187,000 | 611 | 112,000 | 595 units/ (75,000) sq ft |
| Commercial Corridor (CC) | 7.3 | -- | 1.0 | -- | 88,500 | -- | 120,000 | 31,500 |
| Multifamily Flex (MFF) | 36.6 | 30–45 | -- | 733 | -- | 754 | -- | 21 |
| Open Space Recreation (OS-R) | 11.4 | -- | 0.02 | -- | -- | -- | 9,900 | 9,900 |
| Planned Unit Development (PUD-4) | 17.1 | 15 | -- | 254 | | 254 | -- | 254 |
| ROW | 20.5 | -- | -- | -- | -- | -- | -- | -- |
| Total | 120.4 | -- | -- | 1,039 | 277,800 | 1,653 | 241,900 | 614 units/ (35,900) sq ft |

¹Includes a hotel.

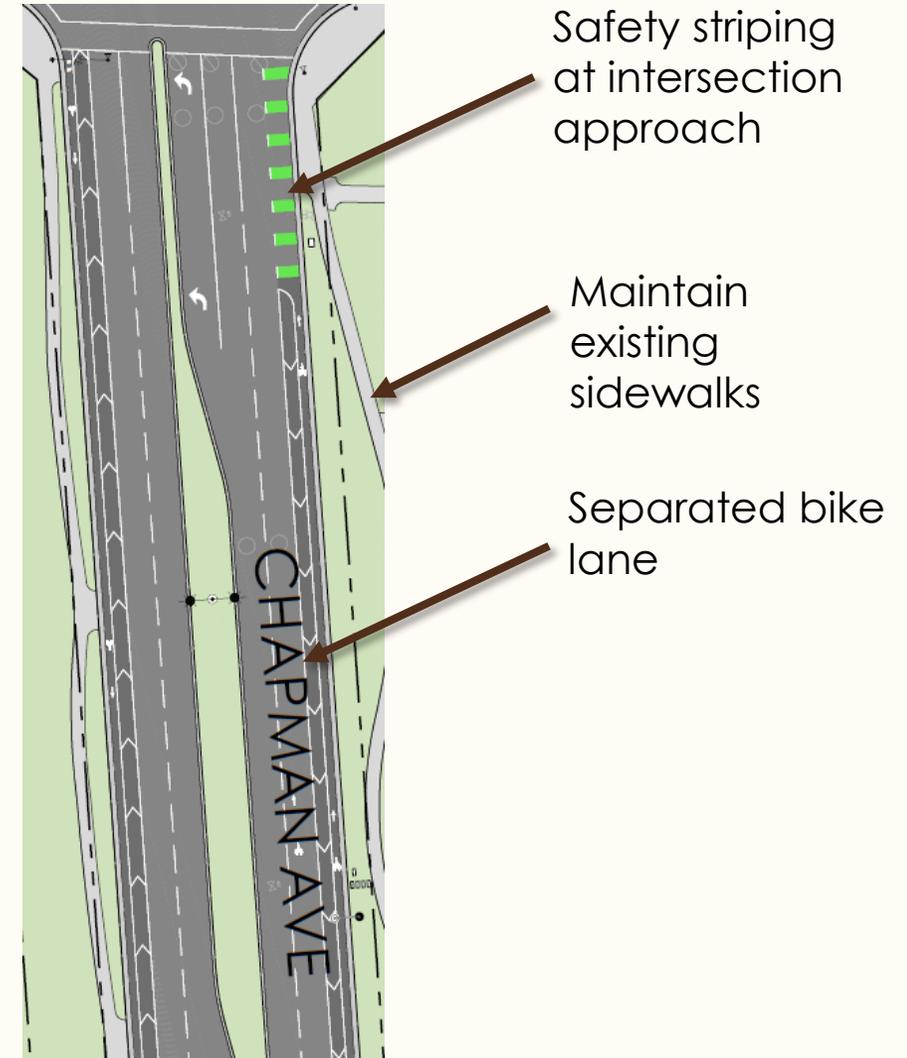
Development and Design Standards

- Standards for new development consider adjacency to single family neighborhoods.
- Regulations include building heights, setbacks, open space, and building form standards.
- Are intended to promote quality design, consistent with the overall vision, while providing a level of flexibility to encourage creative design.
- Design guidelines include additional guidance on building massing and scale, height, setbacks, façade articulation, buildings frontages, materials and finishes.



Mobility and Streetscape Improvements

- Improvements ensure safety for all users, pedestrians, cyclists, and vehicles.
- Long-term changes to Chapman Avenue will help to increase economic activity and beautify the corridor.
- Bicycle improvements include a dedicated lane with vehicular lanes adjusted to accommodate (SB1216).
- On-street parking will be removed from Chapman Avenue.
- **All improvements made within existing right-of-way.**



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Chapman Ave. at Kraemer Blvd.



PERSPECTIVE A1: BEFORE



PERSPECTIVE A1: AFTER

Chapman Ave. Near Kraemer Park



PERSPECTIVE B1: BEFORE



PERSPECTIVE B1: AFTER

Chapman Ave. at Placentia Ave.



PERSPECTIVE C1: BEFORE



PERSPECTIVE C1: AFTER

Streetscape: Trees



Next Steps

- Finalize Infrastructure Analysis (water, sewer, stormdrain)
- Planning Commission Hearing
- Adoption of plan into Municipal Code
- Possible General Plan Amendment
- Adoption of Chapman Avenue Streetscape Master Plan

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Discussion

- **Confirm Previous Council Input** – Integrate feedback from prior study sessions into the draft land use plan.
- **Review Input from Today's Study Session** – Assess and incorporate key takeaways from the current session to refine the plan.
- **Requesting Council Concurrence** – Ensure alignment and agreement on the draft Specific Plan and Streetscape Master Plan.

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DRAFT Streetscape Master Plan



- MASTER PLAN LEGEND**
- EXISTING BUS STOP
 - PROPOSED ACCENT TREE
 - PROPOSED EVERGREEN TREE
 - PROPOSED PALM TREE
 - EXISTING LIGHT POST
 - PROPOSED BIKE LANE
 - BIKE/VEHICULAR LANE MARKING
 - BIKE LANE BUFFER

- C PLACENTIA AVE. ENLARGEMENT
- B KRAEMER PARK ENLARGEMENT
- A KRAEMER BLVD. AND CHAPMAN AVE. ENLARGEMENT

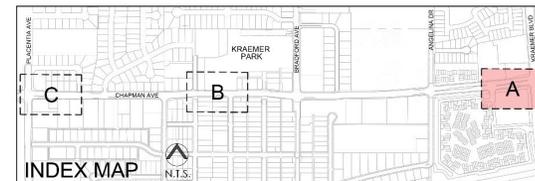


Chapman Corridor Streetscape Master Plan



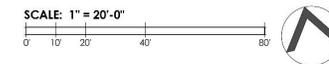
MASTER PLAN LEGEND

-  EXISTING BUS STOP
-  PROPOSED ACCENT TREE
-  PROPOSED EVERGREEN TREE
-  PROPOSED PALM TREE
-  EXISTING LIGHT POST
-  PROPOSED BIKE LANE
-  BIKE/VEHICULAR LANE MARKING
-  BIKE LANE BUFFER

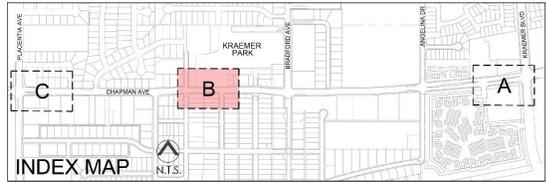


Streetscape Enhancement
Kraemer Blvd and Chapman Ave: A

Chapman Corridor Streetscape Master Plan



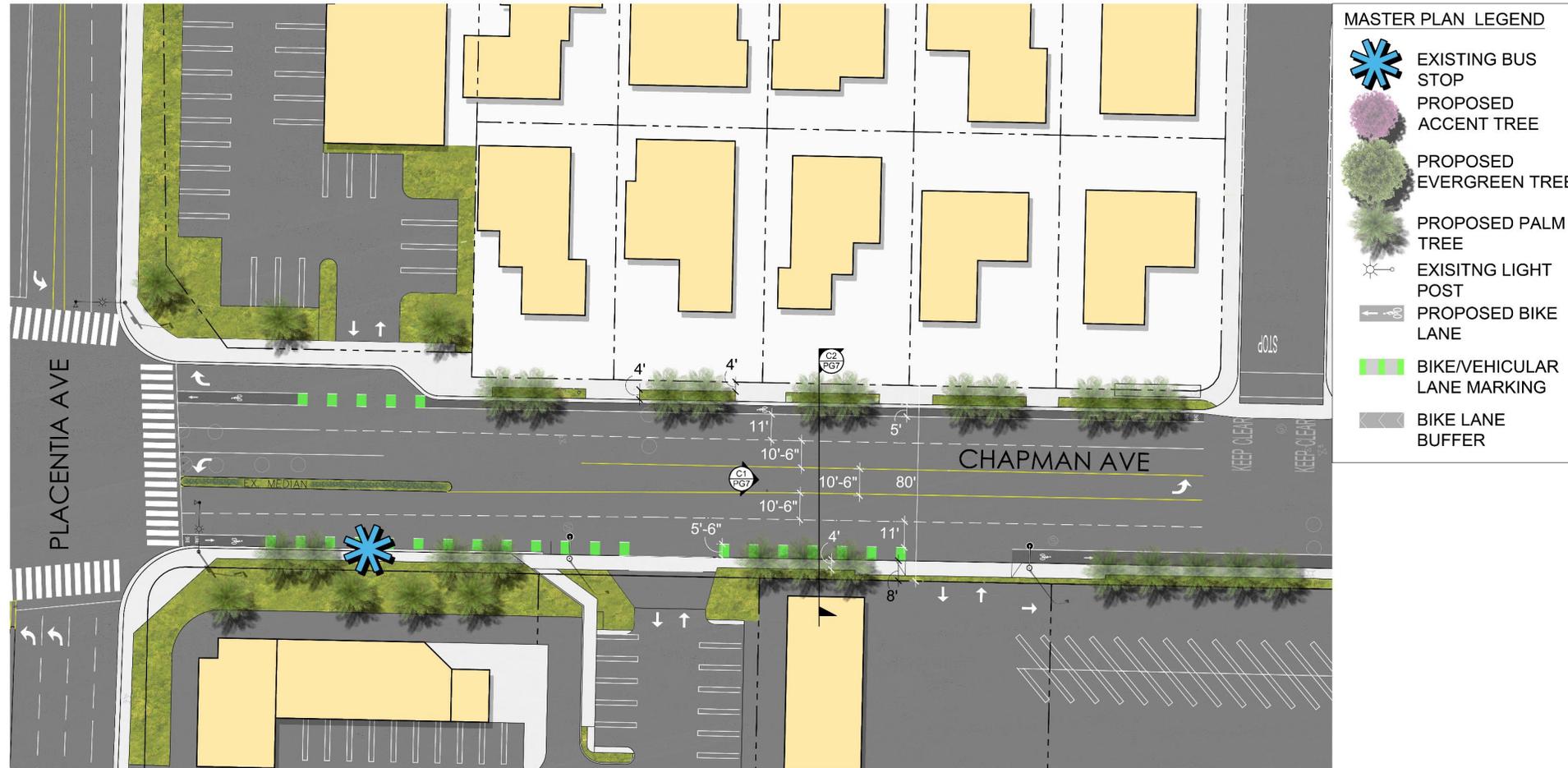
nuvis
LANDSCAPE ARCHITECTURE
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CA 3943, NV 1097, AZ 75045 | SRP, DRP
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SHEET 2 OF 7



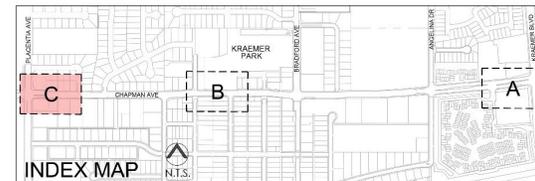
Streetscape Enhancement
Melrose St. and Chapman Ave: B

Chapman Corridor Streetscape Master Plan





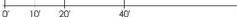
- MASTER PLAN LEGEND**
-  EXISTING BUS STOP
 -  PROPOSED ACCENT TREE
 -  PROPOSED EVERGREEN TREE
 -  PROPOSED PALM TREE
 -  EXISTING LIGHT POST
 -  PROPOSED BIKE LANE
 -  BIKE/VEHICULAR LANE MARKING
 -  BIKE LANE BUFFER



Streetscape Enhancement
Placentia Ave and Chapman Ave: C

Chapman Corridor Streetscape Master Plan

SCALE: 1" = 20'-0"




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ARCHITECTURE
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CA 9943, NV 1097, AZ 7500 | SBE, DBE
24-177.01 03-18-2025
SHEET 6 OF 7



CHAPMAN CORRIDOR REVITALIZATION PLAN

Public Draft: March 2025



City of Placentia
401 E. Chapman Avenue
Placentia, CA 92870

PREPARED BY:



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1 INTRODUCTION

Figure 1-1: Plan Area Map



1.1 Purpose

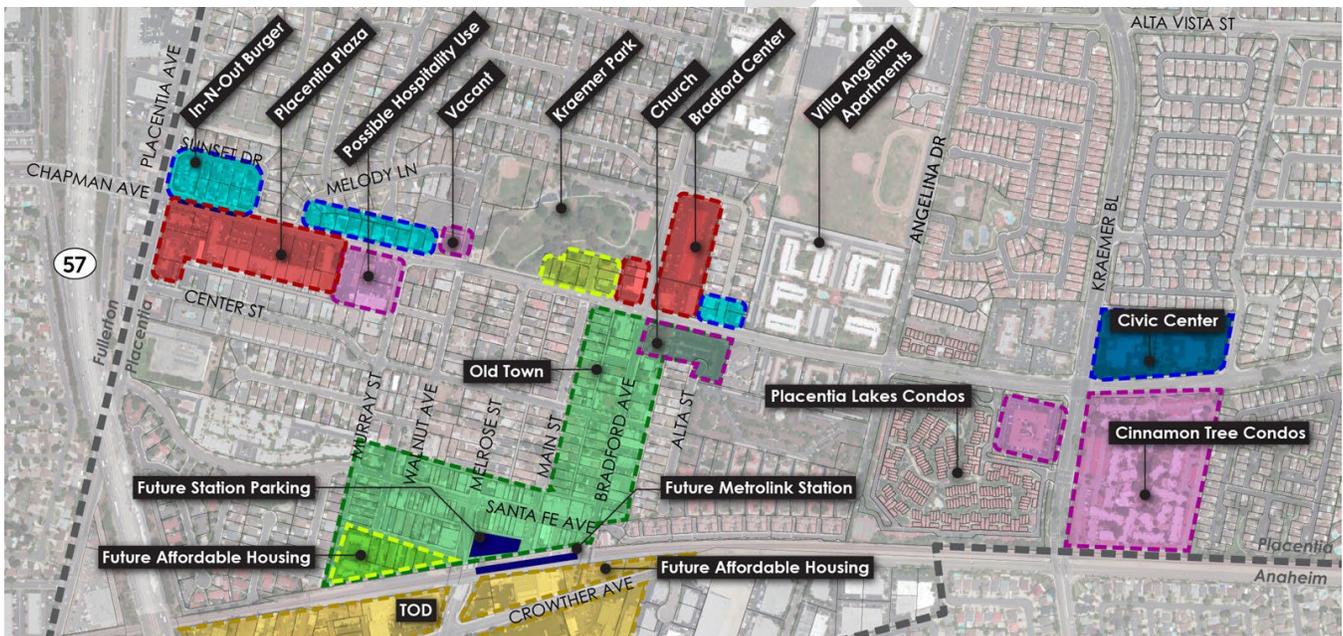
As a gateway to the City of Placentia, the Chapman Corridor Revitalization Plan aims to allow for a vibrant mix of uses and development standards to stimulate the economic and social environment of the Chapman Corridor. Revitalizing this corridor will create an inviting entry that welcomes residents and visitors while providing connections to the adjacent districts and the City's Civic Center. This Plan allows for integrated land use patterns that encourage multimodal travel, walkability, mixed-use development, increased opportunities for residential, public/private open space, and high-quality urban design that is in scale with its respective property and its surrounding area. The Plan encourages a network of cohesive developments that revitalizes the corridor, provides a path to the Old Town and Packing House Districts, and anchors the western city boundary with the City's Civic Center. Development standards and design guidelines (Chapters 4 & 5) are specially tailored for the Chapman Corridor.

As shown on Figure 1-1, *Plan Area Map*, the Plan boundary encompasses a one-mile stretch of Chapman Avenue between Placentia Avenue and the Civic Center and Cinnamon Tree Condos just beyond the intersection of Chapman and Kraemer.

1.2 Background

Placentia is a city with a rich heritage and bright future. The City has already accomplished a tremendous amount of work to build on, with Old Town Placentia and the Transit Oriented Development Packing House Districts both allowing for new mixed-use projects and capitalizing on the forthcoming Metrolink station. The Chapman Corridor is a western gateway to the city and these unique areas. Revitalizing this corridor will create an inviting entry that welcomes residents and visitors while providing a connection to the adjacent districts and the City's Civic Center, as shown on Figure 1-2.

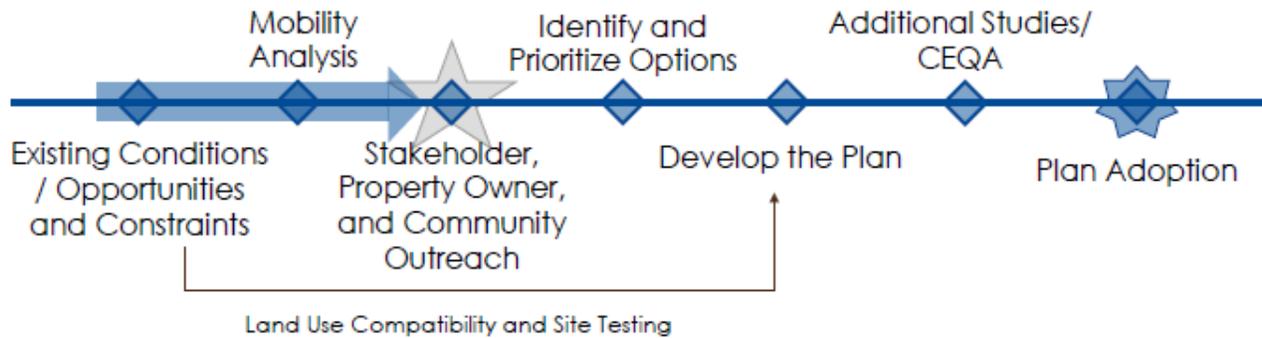
Figure 1-2: Area Context



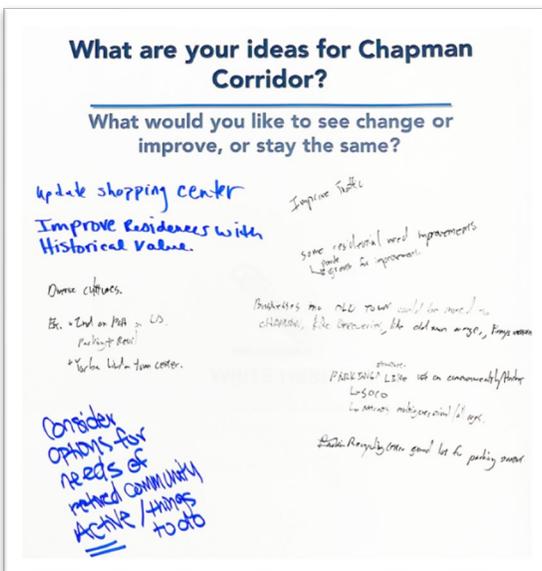
1.3 Community Outreach

During preparation of the Chapman Corridor Revitalization Plan, a series of community meetings and workshops were conducted to gather the community's input and insight (see Figure 1-3). Outreach for the Plan included a community workshop, an online survey, and two City Council study sessions. These community conversations solicited invaluable feedback and data to help inform the vision, values, policies, and design elements of the Plan.

Figure 1-3: Project Process

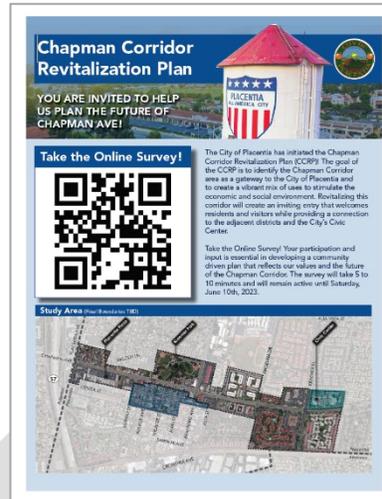


1.3.1 Community Workshop



The City of Placentia hosted a citywide workshop on March 30, 2023, to solicit input from the community on the Chapman Corridor Revitalization Plan. The workshop was held at the Powell Building and was attended by 15 to 20 community members. The workshop consisted of a PowerPoint presentation that included an overview of the project, followed by an open house format with interactive activities and stations. Question prompts for stations included: Where do you live and work? What should the Study Area include? What are your Ideas for Chapman Corridor? What types of uses and buildings would you like to see? and What potential mobility and streetscape improvements would you like to see? A summary of feedback received from the workshop is in Appendix A.





1.3.2 Survey

The Chapman Corridor Revitalization Plan Online Survey was available for three months between March 30, 2023, and July 10, 2023. This survey corresponded to activities that were available during the first Community Workshop held on March 30th. A total of 86 survey responses were received, 85 in English and 1 in Spanish. The survey was promoted on the Plan website, the City Facebook page, at the Placentia Palooza event, and at the City Development Services counter. The survey gathered community feedback on the study area boundaries and ideas for the future of Chapman Corridor and included a visual preference survey of potential land uses and building types on the corridor. A summary of the survey results is in Appendix A.

1.3.3 City Council Study Sessions

Two study sessions were held with City Council to give updates about the project and get input along the way. City staff and the consultant team provided details on community outreach, land plan, and circulation options. Study sessions were held on February 7 and October 3, 2023.

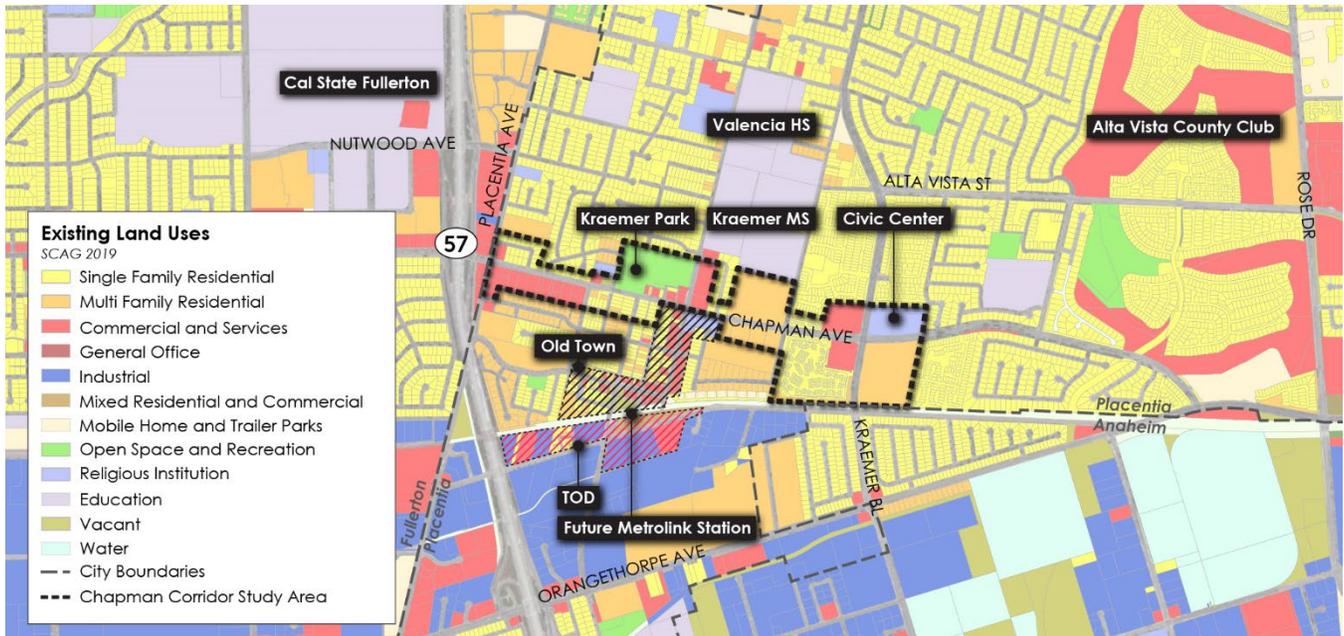
1.4 Setting and Boundary (Existing Conditions)

1.4.1 Existing Land Uses

Figure 1-4, *Existing Land Use Map*, shows existing land uses in the Plan area using data from the Southern California Association of Government (SCAG). The area primarily consists of multifamily residential, single-family residential, and commercial and services uses. Kraemer Park makes up 11 percent of the total area and is centrally located in the corridor (see Figure 1-5).

Key destinations in the Plan area include Northgate Market, In-N-Out Burger, Kraemer Park, and the Placentia Civic Center. Multifamily developments along Chapman Avenue include Villa Angelina, Chapman Woods, and Camino Pueblo and El Rancho apartments.

Figure 1-4: Existing Land Use Map, SCAG 2019



Note: Figure 1-4: Existing Land Use Data is provided by the Southern California Association of Governments at a regional level. Existing Land Use Map is intended to represent existing on the ground uses. For regulatory land use requirements, see Chapter 3 Land Use.

Figure 1-5: Land Use Distribution Chart, SCAG 2019

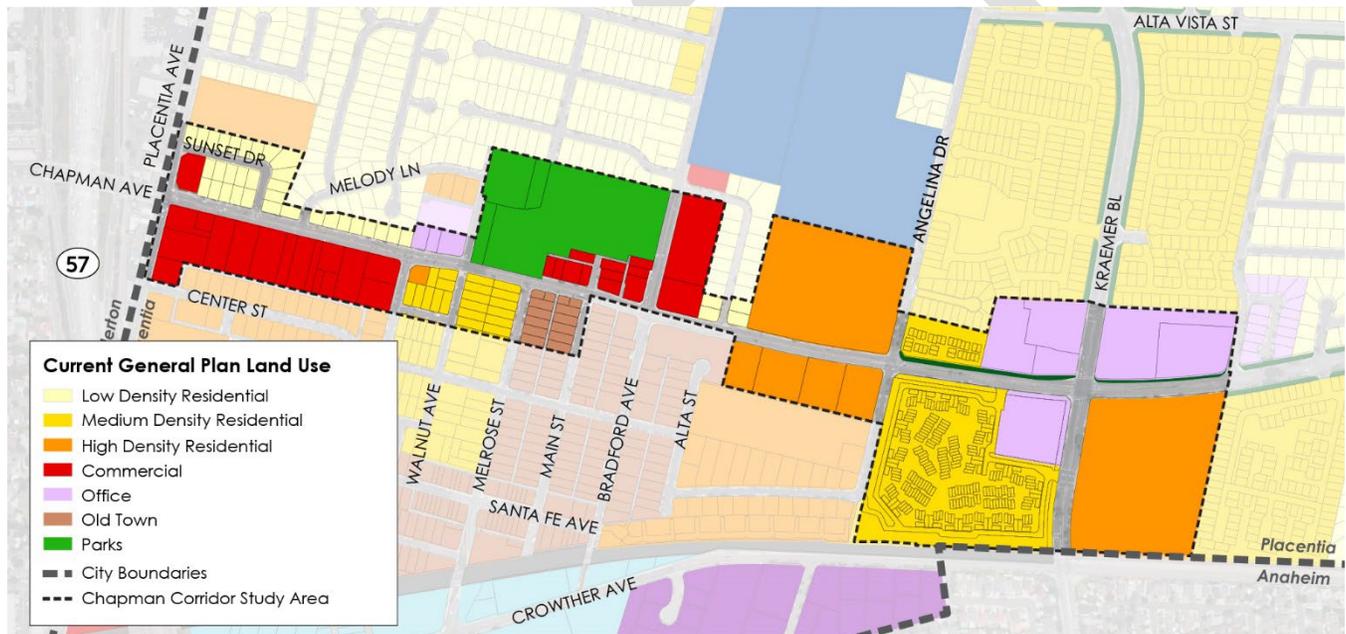


1.5 Relevant Plans and Projects

1.5.1 General Plan

On October 1, 2019, City Council adopted a comprehensive General Plan update. This update included all elements of the General Plan except the housing element, which was updated as part of the required 6th-cycle Regional Housing Needs Assessment. The land use element of the 2019 General Plan strongly encourages enhancements and improvements to the visual image, physical design characteristics, economic vitality, and infrastructure of the Chapman Corridor. This Plan is consistent with the General Plan land use designations and policies (see Figure 1-6).

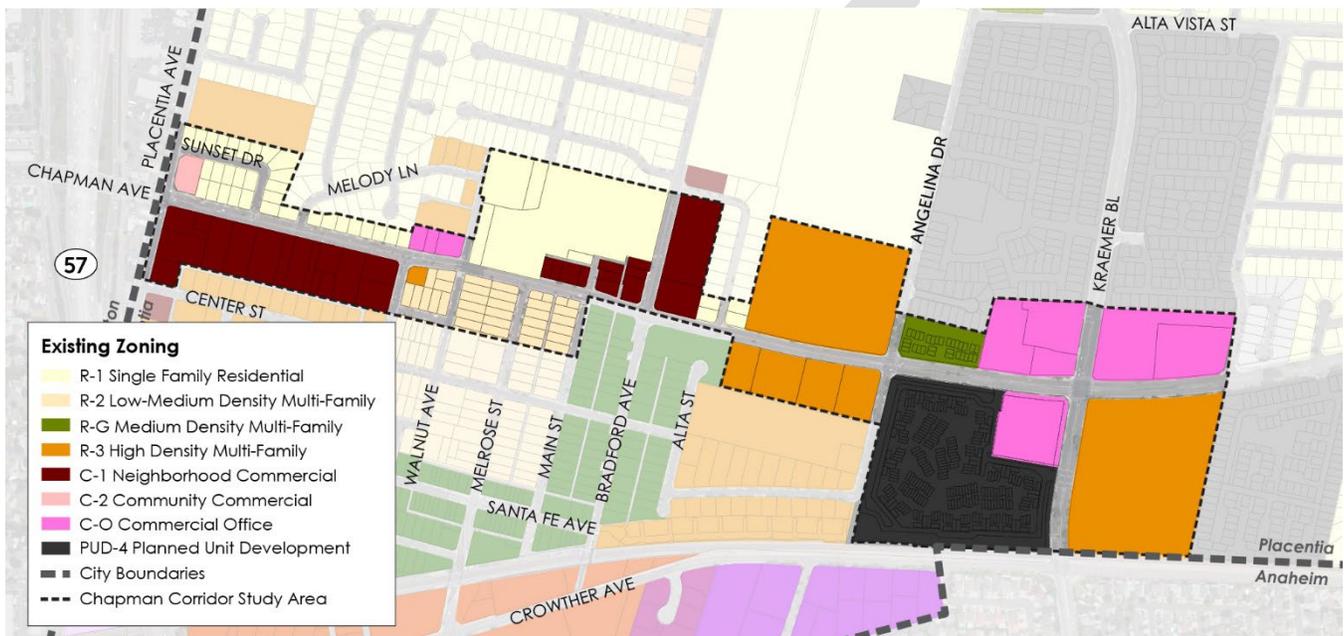
Figure 1-6: Current General Plan Land Use Map, City of Placentia 2024



1.5.2 Current Zoning

The City’s current zoning on Figure 1-7 reflects existing land uses shown on Figure 1-4. This Plan creates new zoning designations for the Chapman Corridor that will replace the existing zoning.

Figure 1-7: Current Zoning Map (2023)



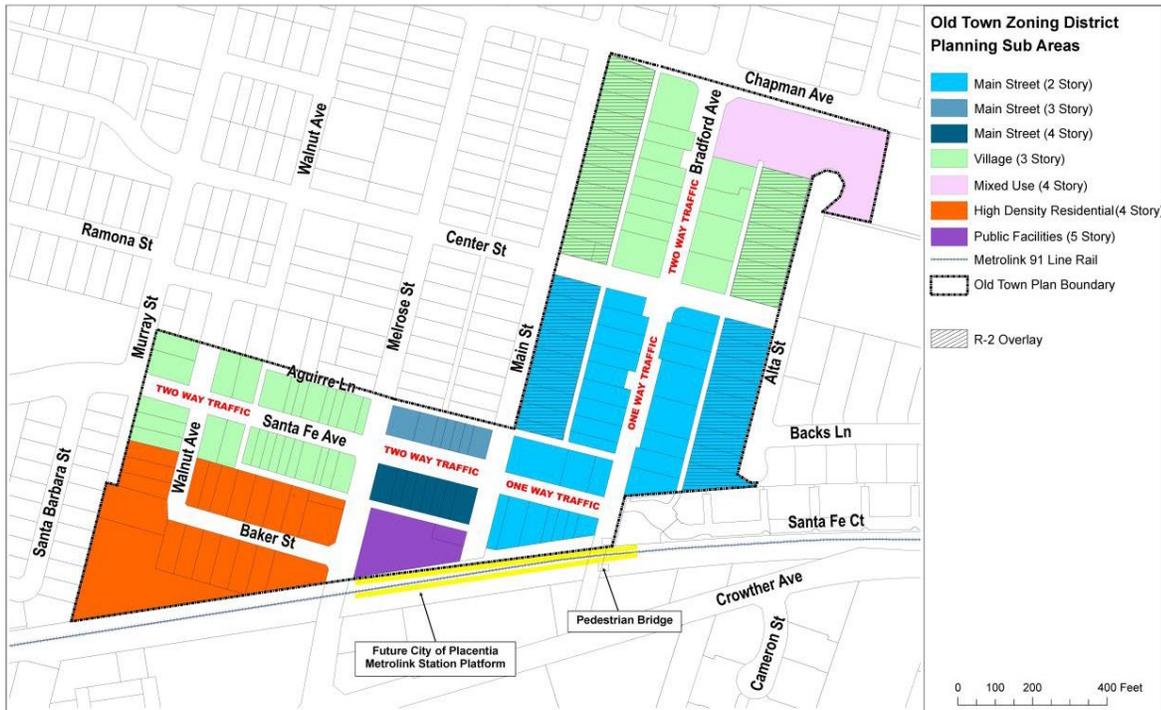
1.5.3 Old Town Revitalization Plan and Streetscape Master Plan

Old Town Revitalization Plan

The Old Town Revitalization Plan, adopted in 2017, is a comprehensive plan for the revitalization, activation, and growth of the City’s original central business district, founded in 1910 near the site of the original Placentia train depot and citrus packing houses. The area sits at the southern edge of the Chapman Corridor Revitalization Plan between Main Street and Alta Street and extends to the future Metrolink Station at Santa Fe Avenue (see Figure 1-8).

Old Town has specific development standards and zoning districts within its boundaries. The area along Chapman Avenue and Bradford Avenue is identified as the Village Subarea. The Village Subarea allows for three stories with setbacks and new walkable retail and office uses. The area east of Bradford Avenue is identified as Mixed-Use Subarea. The Mixed-Use subarea allows for four stories and provides for a mix of concentrated urban housing types, with some commercial uses within convenient walking distance of the Main Street and Village subareas.

Figure 1-8: Old Town Zoning District Planning Subareas



Old Town Streetscape Master Plan

A Streetscape Master Plan for Old Town was prepared concurrently with the Old Town Revitalization Plan. The Old Town Streetscape Master Plan identifies public realm improvements throughout Old Town. Identified improvements include tree well grates, street furniture, pedestrian lights, gateway signage, public art, and decorative traffic signals. Bike lanes are identified on Santa Fe Avenue and Bradford Avenue. Alleyway and parking lot enhancements are also identified to enhance pedestrian connectivity and safety. If a Streetscape Master Plan is developed for Chapman Corridor it should consider consistency with the Old Town Streetscape Master Plan, but site amenities may be unique to the Chapman Corridor to establish a distinct identity.

1.5.4 Enhanced Infrastructure Financing District

The City of Placentia—in partnership with the County of Orange—has established an “enhanced infrastructure financing district” near the city’s future Metrolink station that includes the Old Town Placentia area and Transit Oriented Development Packing House District. The purpose of the financing district is to create a funding mechanism that can facilitate the construction of public infrastructure improvements in this area.

1.5.5 Transit Oriented Development Packing House District and Streetscape Master Plan

Figure 1-9: Transit Oriented Packing House District Project Area



Transit Oriented Development (TOD) Packing House District and Expansion Project

In 2017, the City of Placentia adopted a TOD zoning code and general plan amendment for the Packing House District. In anticipation of the future Metrolink station, the amendment to the zoning code includes specific standards to allow a mixed-use, pedestrian-oriented retail and residential district, creating new development opportunities for a one-of-a-kind destination. This amendment supports retail and restaurant businesses while growing the city's local economy and encouraging mixed-use residential development.

As part of the 2021-2029 housing element update, the City identified the expansion of the original TOD area as a strategy for providing new housing opportunities. The proposed Packing House District TOD Expansion Project will expand the original TOD area to encompass an additional 14.5 acres (TOD expansion area) along Crowther Avenue and allow for the development of up to 1,378 new residential units in the TOD zoning district based on an allowable density of up to 95 units per acre. The City of Placentia is both the project proponent and the lead agency for review of the project under CEQA. The Initial Study/Mitigated Negative Declaration (IS/MND) for the TOD Expansion Project is currently underway.

TOD Packing House District Streetscape Master Plan

The City of Placentia adopted a Streetscape Master Plan concurrently with the TOD Packing House District. The Streetscape Plan identifies streetscape design concepts along Crowther Avenue in the district. Improvements include bike lanes, street furniture, tree grates, landscaping, pedestrian lighting, and gateway signage.

2 VISION & GUIDING PRINCIPLES

2.1 A Vision for the Future of Chapman Avenue

The Chapman Corridor is a prominent gateway into the city, Old Town, and the Packing House District. The street is a vibrant area where the community comes for civic activities at Kraemer Park, the library, and City Hall. Pride of ownership shows throughout the corridor where property owners have upgraded and maintained their shop fronts, homes, and community entries. New mixed-use developments have increased the city’s housing stock and provided cafes and other businesses for residents and visitors. Chapman Avenue itself provides an efficient way to connect to the freeway, Old Town, and the Metrolink Station by driving, taking transit, cycling, or walking. A hotel along the corridor provides an additional lodging option for visitors to Placentia and nearby Cal State Fullerton. Residents who used to drive straight through the area now make a point to stop at their favorite local spot on their way to or from their next destination. This revitalized area anchors the western boundary of the City, signaling to all that they have entered Placentia—“a pleasant place.”

To implement the Plan vision, the proposed land use plan provides flexibility for commercial and residential opportunities to develop. The land use plan aligns with recommendations from the community and City Council and enhances the character of Placentia and Chapman Corridor. Existing uses are allowed to remain; however, new designations create flexibility for potential residences to convert to home businesses and for a variety of mixed-use opportunities throughout the corridor.



2.2 Guiding Principles

- Create subareas to tailor land use standards.
- Create new flexible land use districts allowing for new housing units and mixed-use development.
- Preserve the function of Chapman Avenue but add safe multimodal options.
- Revitalize frontage along Kraemer Park and continue to support civic uses along the corridor.
- Link to Old Town District and Packing House District (TOD).
- Allow for new and expanded retail and commercial opportunities
- Streetscape beautification along Chapman Avenue

3 LAND USE

3.1 Introduction

To implement the Plan vision, the proposed land use plan provides flexibility for commercial and residential opportunities to develop. The land use plan aligns with recommendations from the community and City Council and enhances the character of Placentia and Chapman Corridor. Existing uses are allowed to remain; however, new designations create flexibility for a variety of mixed-use opportunities throughout the corridor.

The Chapman Corridor Plan area is divided into four districts (Figure 3-1). These districts implement various land use designations that provide flexibility for property owners and developers as identified on Figure 3-2. Although a designation may apply within more than one district, the permitted uses and standards (Chapter 4) are further refined in response to adjacent neighborhoods. For example, the multifamily flex designation in the Gateway Core District has a maximum density requirement that is compatible with the surrounding single-family residential uses. Land use designations are defined in Section 3.2. Development standards for each designation by subarea are provided in Chapter 4.

Figure 3-1: Corridor Districts

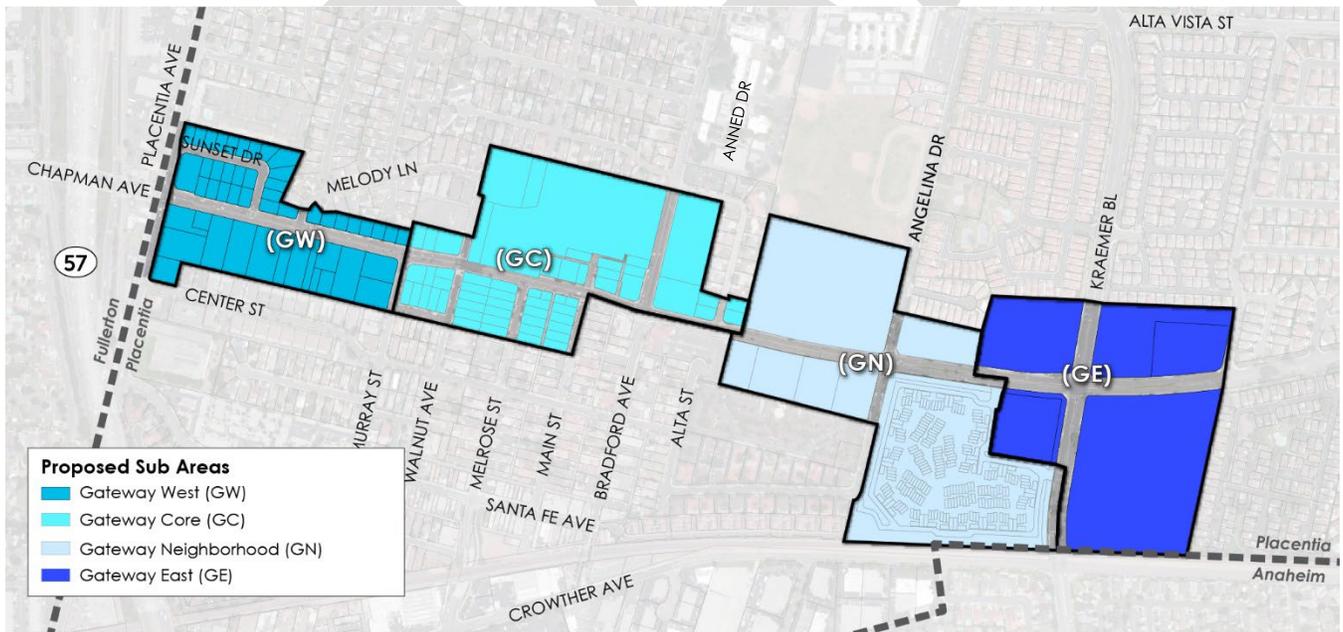
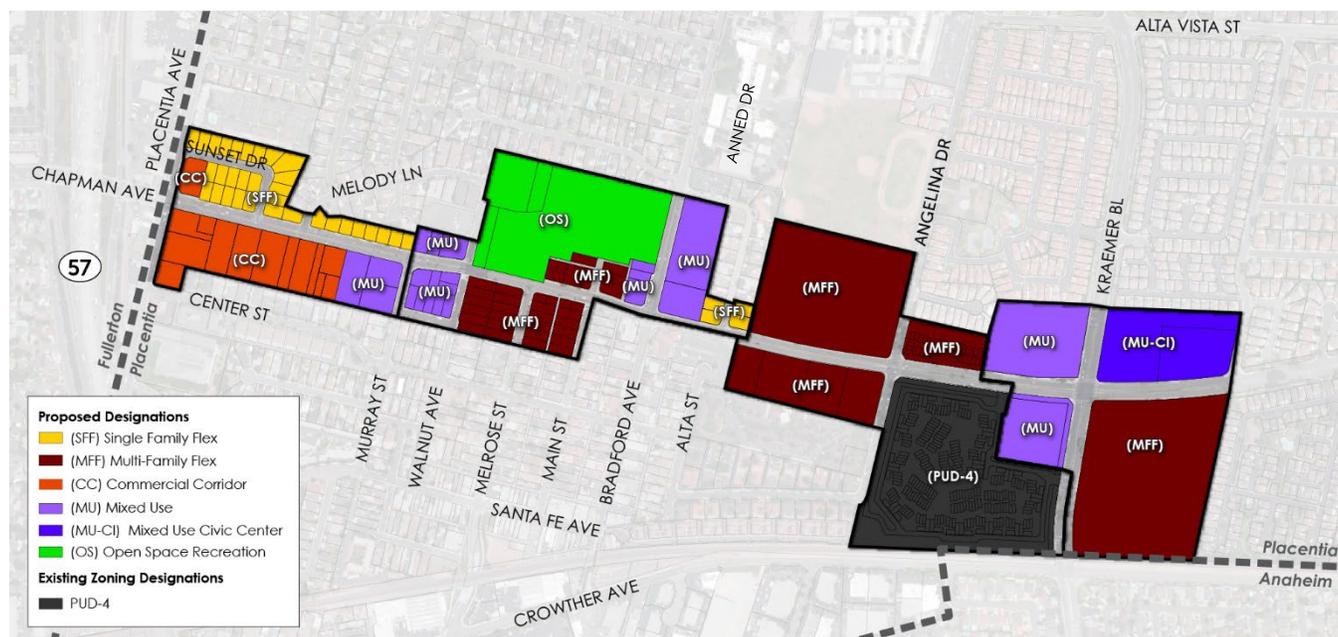


Figure 3-2: Proposed Land Use Plan



3.2 Land Use Definitions

The following land use definitions guide future uses for the corridor and implement the vision for this Plan. Table 3-1 provides the maximum density, intensity, and summary for buildout of the Chapman Corridor Revitalization Plan. The definitions should be used along with Section 3.4, *Land Use and Permit Requirements*.

3.2.1 Single Family Flex (SFF)

The Single Family Flex designation is on existing single-family parcels along Chapman Avenue and Sunset Drive in the Gateway West district and at Anned Drive in the Gateway Core District. The SFF designation would allow existing R-1 zoning and land uses to continue, and also allows adaptive reuse of structures for flexibility of commercial use. For example, residential buildings could be adapted to offices, such as those found on arterial streets in neighboring cities of Orange and Fullerton. The SFF designation would preserve single-family uses, allow flexibility for home business conversion to activate and diversify use types, and maintain the residential character of the neighborhood.

3.2.2 Commercial Corridor (CC)

The Commercial Corridor designation is at the existing In-N-Out, Placentia Plaza shopping center, and car wash at the intersection of Chapman Avenue and Placentia Avenue in the Gateway West District. This designation would allow current commercial uses to continue to operate. This designation preserves commercial uses but increases intensity to incentivize further redevelopment.

3.2.3 Mixed-Use (MU)

The Mixed-Use designation is at the intersections of Chapman Avenue at Murray Street, Walnut Avenue, Bradford Avenue, and Kraemer Boulevard. MU would encourage a diversity of use types along the corridor and accommodate the future catalyst site (Angelina Development) in the Gateway East District and a hotel in the Gateway West District. Depending on the subarea, this designation allows for 2- to 3-story, 3- to 4-story, and 5-story building heights if certain conditions are met. Mixed-use could be vertical or horizontal. There is no requirement for ground-floor commercial or incorporation of retail. Projects may be 100 percent residential or 100 percent commercial.

3.2.4 Multifamily Flex (MFF)

Multifamily Flex designation would allow for multifamily development between 32 and 45 dwelling units per acre, depending on the subarea. This designation will preserve existing residential areas in the Gateway Core, Gateway Neighborhood and Gateway East Districts, as well as encourage multifamily development adjacent to Kraemer Park along Chapman Avenue. Existing uses would be allowed to remain in these areas, and lot consolidation would be required to achieve higher density development.

3.2.5 Mixed-Use Civic Center (MU-CI)

The Mixed-Use Civic Center is at the existing City Hall and Library at the northeast corner of Kraemer Boulevard and Chapman Avenue. The Mixed-Use Civic Center designation would provide flexibility in uses and standards for future use of the Civic Center. This standard would allow for 3- to 4-story buildings, and 5 stories if certain conditions are met.

3.2.6 Open Space Recreation (OS-R)

The Open Space Recreation is at the existing Kraemer Memorial Park. This designation is intended to preserve the existing park and allow for passive and active recreational uses.

3.2.7 Planned Unit Development (PUD-4)

See the Placentia Municipal Code (PMC) Chapter 23.72, "PUD" Planned Unit Development District.

| Table 3-1: Proposed Buildout Statistical Summary | | | | | |
|---|----------------|----------------------------|----------------|--|---|
| Designation | Acreage | Max Density (DU/AC) | Max FAR | Target Residential Dwelling Units | Target Non-residential Square Feet |
| Single Family Flex (SFF) | 5.7 | 6 | | 34 | |
| Gateway West | 5.0 | | -- | 30 | -- |
| Gateway Core | 0.7 | | | 4 | |
| Mixed-Use (MU) | 15.7 | 35-45 | - | 339 | 112,000 |
| Gateway West ¹ | 2.1 | 45 | 1.0 | 35 | 72,000 |
| Gateway Core | 6.1 | 35 | 0.5 | 65 | 26,140 |
| Gateway East ² | 7.5 | 35 | 0.5 | 239 | 13,860 |
| Commercial Corridor (CC) | | | | | |
| Gateway West | 7.3 | | 1.0 | -- | 120,000 |
| Multifamily Flex (MFF) | 36.6 | 30-45 | | 754 | |
| Gateway Core ² | 5.0 | 30 | -- | 80 | -- |
| Gateway Neighborhood | 17.8 | 45 | | 400 | |
| Gateway East | 13.8 | 35 | | 274 | |
| Mixed-Use Civic Center (MU-CI) | | | | | |
| Gateway East | 6.0 | 45 | 0.35 | 272 | -- |
| Open Space Recreation (OS-R) | | | | | |
| Gateway Core | 11.4 | -- | .02 | -- | 9,990 |
| Planned Unit Development (PUD-4) | 17.1 | 15 | -- | 254 | -- |
| ROW | 20.5 | -- | -- | -- | -- |
| Total | 120.4 | -- | -- | 1,653 | 241,900 |

Source: Economic Demand Study, Kosmot, 2024.

NOTE: Buildout is estimated for properties that are most likely to change use or redevelop.

1. Building square feet includes a hotel.
2. City Owned Property and Catalyst Site may develop at a higher density if the criteria in Section 3.3 are met.

3.3 Opportunity Sites

Several properties along the corridor are likely to be redeveloped in the near-term. These sites as well as possible reuse plans are discussed below.



Rendering of potential catalyst site project. Credit: Kraemer Land Company (2021)

3.3.1 Catalyst Site

As is the case in the City's TOD zone, a developer could take advantage of the benefits of a designated catalyst site. A catalyst site is defined as the first entitled project in the Plan area and meets the following criteria:

- a) It must be a minimum of one acre and shall contain no less than 65 dwelling units per acre.
- b) The catalyst site may exceed maximum allowed building height by one floor.
- c) If the first entitled project is withdrawn after entitlement, the next entitled project may be identified as a catalyst site under discretion of the Planning Director.

A catalyst project has been identified on a 2.96-acre parcel at the southwest corner of Chapman Avenue and Kraemer Boulevard. This mixed-use project proposes 239 new units at approximately 81 dwelling units per acre, a 2,310-square-foot café, and 5,240 square feet of retail office space. The proposed project is six stories high, and parking is in the first two stories. The City of Placentia has identified this project as a catalyst site.

3.3.2 Potential Multifamily Housing with Affordable Housing Incentive

Figure 3-3: Potential Multifamily Housing Illustrative



The City has identified parcels at the intersection of Chapman Avenue and Main Street, adjacent to Kraemer Park, as an opportunity site for potential multifamily affordable housing. The area includes single-story commercial office buildings, vacant land, the Placentia Chamber of Commerce, and Suzie’s Freeze. The appliance store at Bradford Avenue is expected to remain. Specific parcels are currently owned by the City.

The Plan designates this area as Multifamily Flex in the Gateway Core Subarea. Figure 3-3 illustrates one example of potential development at this opportunity site. This development assumed consolidation of all parcels. To further maximize density, the potential development also utilizes public right-of-way at Main Street north of Chapman Avenue. Potential future residents would benefit from the adjacency to the park, and the land use typology and density is compatible with the density of the existing neighborhood. These properties can benefit from an affordable housing incentive if a developer provides 25% of

the units as deed restricted affordable housing. This incentive is flexible and does not require a specific income level. If 25% of the units proposed for this site are set aside for Very Low, Low, and/or Moderate-income households as defined by the City’s Housing Element, then the project may develop up to 45 dwelling units an acre.

Figure 3-4: Potential Hotel Illustrative



3.3.3 Potential Hotel

At the southwest corner of Chapman Avenue and Murray Street is a strip-mall with retail and services and a Fantasy Burger restaurant. West of these uses is a recycling center and vacant office buildings. These combined sites are envisioned as a small boutique hotel that would accommodate guests for Chapman Corridor, Old Town, the Packing House District, and surrounding communities.

3.4 Land Use and Permit Requirements

New development, including additions to existing buildings, shall comply with the provisions of Ch. 23.75, and shall follow the entitlement procedures therein.

- 1) Allowable Land Uses. A parcel or building within Chapman Corridor shall be occupied by only the land uses allowed by Table 3-2, Land Use and Permit Requirements. Each land use in the table is defined in the glossary of the ordinance codified in PMC Chapter 23.04, Definitions.
- 2) Multiple Uses. Any one or more land use identified by Table 3-2 as being allowable within Chapman Corridor may be established on any parcel, subject to the planning permit requirement listed in the table, and in compliance with all applicable requirements of this code.
- 3) Unlisted Uses. The development services director or designee may determine an unlisted use is similar to another allowable permitted or conditionally permitted use if all of the following findings can be made:
 - a. The use is no greater in density or intensity than other uses allowed or conditionally allowed in the planning subarea.
 - b. The use is compatible with permitted or conditionally permitted uses in the planning subarea.
 - c. The use will meet the purpose of the planning subarea.
 - d. The use is consistent with the goals and policies of the general plan.
 - e. The use will not be detrimental to the public health, safety, or welfare.

Applicants may appeal this decision using the use conformity determination process, outlined in Section 23.39.035 of the PMC.

- 4) Permit Requirements. Table 3-2 provides for land uses that are:
 - a. Permitted. These uses are permitted subject to compliance with all applicable provisions of this chapter and may require a development plan review or site plan review in compliance with Chapter 7 of this Plan. These uses are shown as “P” uses in Table 3-2.
 - b. Conditionally Permitted Uses. These uses are allowed subject to the approval of a use permit and require a public hearing before the planning commission in compliance with Chapter 23.87 of the PMC. These uses are shown as “UP” in Table 3-2. All proposed projects with use permits in the Plan area must be reviewed by the City Council Housing, Community, and Economic Development Committee; see Chapter 7, *Administration and Implementation*.
 - c. Not Permitted. These uses are not permitted and are shown as “NP” in Table 3-2. A use that is not listed in Table 3-2 is not allowed in the district, except as otherwise provided in Section 3.4 (3) Unlisted Uses of this Plan. Uses that are expressly listed as not permitted are prohibited.

- d. Standards for Specific Land Uses. Where the last column in Table 3-2 (Specific Use Regulations) includes a section number, the regulations in the referenced section of this chapter and/or the PMC apply to the use. Provisions in other sections of this chapter may also apply.

5) In any case this Plan is silent, the applicable sections of the PMC shall apply.

3.5 Land Use Table

| Table 3-2: Land Use and Permit Requirements | | | | | | | |
|---|-----------------------|-----|----|----|----|-----------|---|
| Permit Requirement P: Permitted UP: Use Permit NP: Not Permitted | Land Use Designations | | | | | | Notes and Special Use Regulations by Subarea |
| | SFF | MFF | CC | OS | MU | MU- CI | |
| Recreation, Education, Public Assembly Uses | | | | | | | |
| Commercial Recreation Facility, Indoor | NP | NP | UP | NP | NP | NP | |
| Community Recreation Facility | NP | NP | NP | P | NP | UP* | Recreation Facility allowed in MU-CI in Gateway East subarea only through a Use Permit. |
| Conference/Convention Facility | NP | NP | NP | NP | NP | NP | |
| Health/Fitness Facility, including stand-alone or roving fitness classes | UP | NP | P | NP | P | P | |
| Library, Museum | P | NP | NP | P | P | P | |
| Public Park, Playground or Plaza | P | NP | P | P | P | P | |
| School: specialized education, training | UP | NP | NP | NP | NP | NP | |
| Studio: art, dance, martial arts, music, photography, cooking, instructional, fitness (yoga, Pilates, spin, etc.) | UP | NP | P | UP | P | P | Not to exceed 1,500 SF |
| Theater (live performing arts, live music) | NP | NP | P | UP | UP | UP | |

| Table 3-2: Land Use and Permit Requirements | | | | | | | |
|---|-----------------------|-----|----|----|----|-----------|--|
| Permit Requirement P: Permitted UP: Use Permit NP: Not Permitted | Land Use Designations | | | | | | Notes and Special Use Regulations by Subarea |
| | SFF | MFF | CC | OS | MU | MU- CI | |
| City Center (City Hall and related civic uses) | NP | NP | NP | P | NP | P | |
| Residential Uses | | | | | | | |
| Emergency/Transitional Shelter | NP | NP | NP | NP | NP | NP | |
| Home Occupation | P | P | NP | NP | P | P | Must comply with home occupation provisions of PMC. 23.81.020 |
| Live-Work | NP | NP | NP | NP | UP | UP | |
| Multifamily Residential | NP | P | NP | NP | P | P | Includes apartments, condominiums, courtyard housing, and townhomes. |
| Assisted Living Facility | NP | NP | NP | NP | NP | NP | |
| Single-Family (existing only) | P | NP | NP | NP | NP | NP | |
| Mixed-Use with Residential Development | NP | NP | NP | NP | P | P | |
| Retail/Commercial Uses | | | | | | | |
| Accessory Retail or Services | NP | NP | UP | NP | P | P | Only permitted when primary commercial use is established. |
| Adult Entertainment Facility or Business | NP | NP | NP | NP | NP | NP | Pursuant to PMC Ch 23.89. |

| Permit Requirement P: Permitted UP: Use Permit NP: Not Permitted | Land Use Designations | | | | | | Notes and Special Use Regulations by Subarea |
|--|-----------------------|-----|----|----|----|-------|---|
| | SFF | MFF | CC | OS | MU | MU-CI | |
| Alcoholic Beverage Sales (not associated with bar, brewery, distillery, restaurant, or neighborhood market or grocery) | NP | NP | NP | NP | NP | NP | “Alcoholic beverages” means any spirituous, vinous, malt, or other alcoholic liquor. |
| Retail Sales of Fine Wine and Beer | NP | NP | UP | NP | UP | NP | “Fine Wine and Beer” means all types of wine and malt beverages for consumption off the premises in original, sealed containers. No on-site consumption allowed. |
| Antique or Collectible Store | P | NP | P | NP | P | P | |
| Artisan/Handcraft Shop | P | NP | P | NP | P | P | |
| Auto Repair, Auto Sales, or Auto Parts Sales | NP | NP | NP | NP | NP | NP | |
| Bar, Tavern, Brewery, Distillery, Tasting Rooms, Wine Cellar | NP | NP | P | NP | UP | UP | |
| Convenience Store (with alcohol sales) | NP | NP | UP | NP | UP | UP | No on-site consumption allowed. |
| Drive-Through (any uses) | NP/UP* | NP | UP | NP | NP | NP | Use Permit in Gateway West and Gateway Core Subareas Only. Minimum aggregated lot size 18,000 SF. Lot consolidation must be in combination with an existing commercial use. |
| Furniture, Furnishings, and Appliance Store | NP | NP | P | NP | P | P | |
| General Retail: less than 5,000 sf | P | NP | P | NP | P | P | |
| General Retail: 5,000 sf to 20,000 sf | UP | NP | P | NP | UP | UP | |

| Table 3-2: Land Use and Permit Requirements | | | | | | | |
|---|-----------------------|-----|----|----|----|-------|--|
| Permit Requirement P: Permitted UP: Use Permit NP: Not Permitted | Land Use Designations | | | | | | Notes and Special Use Regulations by Subarea |
| | SFF | MFF | CC | OS | MU | MU-CI | |
| General Retail: more than 20,000 sf (max 60,000 sf) | NP | NP | P | NP | UP | UP | |
| Groceries, Specialty Foods: 10,000 sf or less | NP | NP | P | NP | P | P | With alcohol sales, a use permit is required. |
| Groceries, Specialty Foods: more than 10,000 sf | NP | NP | UP | NP | UP | UP | |
| Medical Marijuana Facilities | NP | NP | NP | NP | NP | NP | Pursuant to PMC Ch 23.46. |
| Neighborhood Market (without alcohol beverage sales): 10,000 sf or less | NP | NP | P | NP | P | P | With alcohol sales, a use permit is required. |
| Nightclub | NP | NP | NP | NP | NP | NP | |
| Outdoor Dining | UP | NP | P | NP | P | P | With alcohol sales, a use permit is required. Outdoor dining is allowed by approval of the director of development services and director of public works or their designees and may require Alcoholic Beverage Control Board (ABC) approval. Approval is pursuant to the outdoor dining permit and guidelines. |
| Outdoor Display and Sales | NP | NP | P | NP | NP | NP | May not encroach into public right-of-way. |
| Pet Store, Pet Boarding, Animal Shelter | NP | NP | P | NP | P | P | Sheltering and boarding shall require a use permit. |

| Table 3-2: Land Use and Permit Requirements | | | | | | | |
|---|-----------------------|-----|----|----|-----|-------|---|
| Permit Requirement P: Permitted UP: Use Permit NP: Not Permitted | Land Use Designations | | | | | | Notes and Special Use Regulations by Subarea |
| | SFF | MFF | CC | OS | MU | MU-CI | |
| Recreation Entertainment | NP | NP | UP | UP | UP | UP | 20,000 SF max. |
| Restaurant | NP/UP* | NP | P | NP | P | P | *For SFF Use-Permit in Gateway West Subarea only with a minimum aggregate lot size 18,000 SF. Lot consolidation must be in combination with an existing commercial use. |
| Café | UP | NP | P | UP | P | P | |
| Restaurant with Alcohol Sales | UP | NP | P | NP | P | P | Pursuant to Alcoholic Beverage Control Board (ABC) approval. |
| Pawn Shop | N | N | N | N | N | N | |
| Service Station | NP | NP | UP | NP | UP* | NP | Mixed Use: Use-Permit in Gateway Core only. Must be on corner lot and 15,000 SF minimum. |
| Tobacco Sales, including Electronic Smoking Devices | NP | NP | P | NP | UP | UP | |
| Services: Business, Financial, Medical, Professional | | | | | | | |
| ATM | NP | NP | P | NP | P | P | Must be integrated into building façade. Stand-alone kiosks not permitted. |
| Bank (2,000 sf or less) | NP | NP | P | NP | NP | NP | Banks over 2,000 SF are not permitted. |
| Business Support Services (copying, printing, office supplies, etc.) | UP | NP | P | NP | P | P | |
| Hospital | NP | NP | NP | NP | NP | NP | |
| Medical Office | UP* | NP | P | NP | P | NP | Single Family Flex: No new construction allowed. Use must occupy existing building. |

| Table 3-2: Land Use and Permit Requirements | | | | | | | |
|---|-----------------------|-----|-----|----|--------|-------|---|
| Permit Requirement P: Permitted UP: Use Permit NP: Not Permitted | Land Use Designations | | | | | | Notes and Special Use Regulations by Subarea |
| | SFF | MFF | CC | OS | MU | MU-CI | |
| Urgent Care | NP | NP | P | NP | P | NP | |
| Office | UP* | NP | P | NP | P | P | Single Family Flex: No new construction allowed. Use must occupy existing building. |
| Services – General | | | | | | | |
| Adult Daycare | P | NP | NP | NP | NP | NP | |
| Cigar or Hookah Lounge | NP | NP | UP | NP | NP | NP | |
| Commercial Daycare Center | UP | NP | P | NP | NP | NP | |
| Hostess Bars | NP | NP | NP | NP | NP | NP | |
| Laundromat and Laundry Services | NP | NP | UP | NP | UP | NP | |
| Lodging: B&B | NP | NP | NP | NP | NP | NP | |
| Lodging: Hotel | NP | NP | NP | NP | UP* | NP | Allowed in Gateway West Mixed-Use Only |
| Massage Establishments | NP | NP | UP | NP | UP | NP | |
| Meeting Halls, Banquet Centers | NP | NP | UP* | NP | UP/NP* | NP | Commercial Corridor: Must be associated with restaurant facilities; Gateway West Mixed-Use Only: Must be associated with Hotel use or restaurant facility; All other mixed-use not permitted. |
| Personal Services | NP | NP | P | NP | P | P | |
| Public Safety Facility | NP | NP | NP | P | NP | P | |
| Spa Services (with or without alcohol) | NP | NP | UP | NP | UP | UP | Pursuant to Alcoholic Beverage Control Board (ABC) approval |
| Tattoo Parlors/Body Modification | NP | NP | UP | NP | UP | UP | |

| Table 3-2: Land Use and Permit Requirements | | | | | | | |
|---|-----------------------|-----|----|----|----|-------|---|
| Permit Requirement P: Permitted UP: Use Permit NP: Not Permitted | Land Use Designations | | | | | | Notes and Special Use Regulations by Subarea |
| | SFF | MFF | CC | OS | MU | MU-CI | |
| Transportation, Communications & Infrastructure | | | | | | | |
| Broadcasting or Recording Studio | UP* | NP | UP | NP | UP | UP | Single Family Flex: No new construction allowed. Use must occupy existing building. No roof- or ground-mounted antenna or broadcasting apparatus allowed that is visible from street, adjacent properties, or otherwise detracts from the existing residential character. |
| Parking Structure or Surface Parking Lot (standalone) | NP | NP | UP | UP | UP | UP | Pursuant to PMC 23.78 |
| Transit Station or Terminal | NP | NP | NP | NP | NP | NP | |
| Telecommunication Cell Tower/Microcell and Small Cell Sites | UP | UP | UP | UP | UP | UP | Pursuant to PMC Ch 23.82; Must be on building rooftop. Macro telecommunication facilities are not permitted |

For PUD-4, refer to PMC Chapter 23.72.

4 DEVELOPMENT STANDARDS

4.1 Introduction

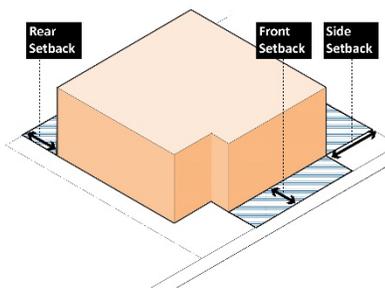
This chapter establishes land use and design requirements for the Chapman Corridor Revitalization Plan (CCRP) area. These regulations include building heights, setbacks, parking requirements, open space, and building form standards. The standards are consistent with and implement the applicable goals and policies of the City's General Plan pertaining to this area as well as the vision and guiding principles established in this Plan. This chapter should be used in conjunction with Chapter 5, *Design Guidelines*, when reviewing and processing development applications for properties in the CCRP area.

The following standards are organized by land use category and then broken down further by designation and subarea as follows:

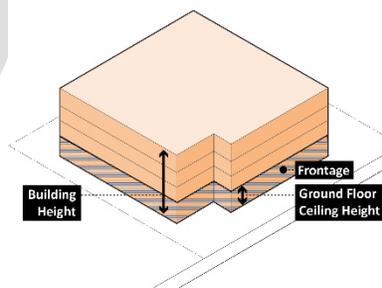
- Residential: Single Family Flex, Multi Family Flex
- Non-residential: Commercial Corridor
- Mixed-Use: Mixed-Use, Mixed-Use Civic Center

Definitions, permit requirements, and permitted uses for each land use category are provided in Chapter 3, *Land Use*. The development standards of this Specific Plan are intended to replace City zoning regulations except where noted. Where the Specific Plan is silent, the Placentia Municipal Code shall apply. The standards of the CCRP shall not apply to development for which approvals were granted prior to the adoption of these regulations and for which entitlements are still valid and for development that has current, valid building permits.

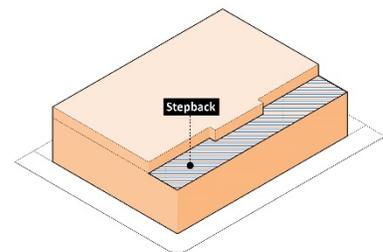
Throughout this chapter, standards are provided by land use designation and further refined by subarea. The standards address setbacks, heights, and in some cases stepbacks. The illustrations, below, are provided for reference for building height and setback standards. Additional standards that apply to all zones in the CCRP area are provided at the end of this chapter.



Setbacks



Height



Stepbacks

4.2 Residential Development Standards

4.2.1 Single Family Flex

The Single Family Flex designation occurs in two subareas.

Gateway West (GW). Properties in the Gateway West subarea are encouraged and anticipated to convert to non-residential uses in the current structure, as described in Chapter 3. Suitable adaptive reuse of these homes include uses such as coffee shops, salons, and small medical offices such as dental. Single-family homes that remain as residential use are a conforming use and may remain. The standards in Table 4.1 shall apply.

Gateway Core (GC). Properties in the Gateway Core subarea are encourage to maintain their single-family use, and the standards in Table 4.1 shall apply.

| Subareas | GW | GC |
|-----------------------------------|---|----|
| Density (max du/ac) | 6* | 6 |
| Building Height | Main building: 30 feet max. | |
| Setbacks (General) | All of the required setback area shall be landscaped and maintained unless otherwise allowed. | |
| Setbacks Front | 20 feet min. | |
| Setbacks Side | 6 feet min each side. | |
| Setbacks Rear | <ul style="list-style-type: none"> a) 20 feet minimum from rear property line. b) Room Additions, Accessory Structures and Enclosed Patios: 10 feet min from rear property line. c) Patio Covers: 5 feet min from rear property line, as measured from the overhang of the patio cover. | |
| Allowable Setback Projections | <ul style="list-style-type: none"> a) Ground Floor. <ul style="list-style-type: none"> i. Awnings and canopies over windows: 60 inches maximum. ii. Shade structures: 15 feet maximum. iii. Bay windows: 3 feet maximum iv. Cornices, belt courses, and similar architectural features: 12 inches maximum. v. Eaves, roof overhangs: 30 inches maximum. vi. Uncovered porches, decks, and landings (may be covered by arbors or trellises): 10 feet maximum. b) Above Ground Floor. Awnings, galleries, balconies, bay windows: 48 inches maximum. c) Signs, see section 4.6. d) All projections must maintain a minimum of 8 feet vertical clearance from ground. | |
| Public Right-of-Way Encroachments | <ul style="list-style-type: none"> a) Art, as determined by the approval of the public art component of the project. b) Signs, see section 4.6. c) NOTE: Requires approval of an encroachment permit. | |

| Table 4.1: Single-Family Flex Development Standards | | |
|--|---|---|
| Subareas | GW | GC |
| Lighting | All lighting, exterior and interior, shall be designed and located so as to confine direct rays to the premises. | |
| Building Orientation | Buildings shall be oriented to the public street. | Buildings in this subarea are not required to front Chapman Avenue. |
| Commercial Uses, General | Only allowed on parcels fronting Chapman Avenue. All development standards above shall apply to commercial uses in the GW zone. | Not allowed. |
| Commercial Uses, Commercial Conversion | For commercial conversion of former residential properties, additions required for the purpose of complying with building and/or fire safety codes are exempt from setback requirements (e.g., new area for ADA access or egress). | Not allowed. |
| Outdoor Dining | <ul style="list-style-type: none"> a) Outdoor dining allowed in conjunction with café or restaurant use only. b) Alcoholic beverages may be served in an outdoor eating area of an eating establishment provided the business operating the outdoor eating area has obtained an approval authorized by this Plan and the PMC to serve alcoholic beverages on-site and has obtained and maintains all approvals required by the ABC. c) Outdoor dining must be within the front setback area or rear yard and may not encroach into the public right-of-way. d) Background music and speakers, and live entertainment are prohibited in Outdoor Dining Areas, unless otherwise approved by the City. e) Outdoor dining areas shall not operate no later than 10:00 p.m. if abutting a residential zone, unless otherwise approved by the City. f) See Chapter 5 Section 5.15 Outdoor Dining of this Plan for design related standards. | Not Allowed |

Table 4.1: Single-Family Flex Development Standards

| Subareas | GW | GC |
|--|----|----|
| <p>NOTES: Existing single-family homes in the Single Family Flex district may be converted to a non-residential use such as a medical, dental, or professional office if the site can support the parking requirements of PMC 23.78.</p> <p>If the standards of the zone are silent on a particular topic, the R-1 standards of the PMC shall apply.</p> | | |

4.2.2 Multi-Family Flex

The Multi-Family Flex designation occurs in three subareas.

Gateway Core (GC). Properties in the Gateway Neighborhood subarea are adjacent to Old Town and single-family neighborhoods where additional standards provide a transition between buildings that vary in size and height, preserving privacy for all residents. Lot consolidation within this subarea is encouraged to provide multi-family units at a size and scale that is feasible for new development. The standards under GC in Table 4.2 shall apply.

Gateway Neighborhood (GN). Properties in the Gateway Neighborhood subarea are existing condo and apartment communities that are already built out and not expected to change. These areas are adjacent to single-family neighborhoods, and if properties within this subarea were to redevelop, additional standards reinforce a transition between buildings that vary in size and height, preserving privacy for all residents. The standards under GN in Table 4.2 shall apply.

Gateway East (GE). The Gateway East subarea includes Cinnamon Tree Condominium Community. The standards under GE in Table 4-2 are intended to guide this property for redevelopment or future improvements.

| Table 4.2: Multi-Family Flex Development Standards | | | |
|---|--|-------------------------------------|-----------|
| Subareas | GC | GN | GE |
| Density (max du/ac) | 30 | 45 | 35 |
| Building Height | 35 feet max. / 3 stories. 30 feet max if adjacent to Single-Family homes; however, 35-foot maximum height shall be permitted when 2 feet of additional setback is provided for every foot of additional height. | 40 feet max. 3 stories. | |
| Lot Size | 10,000 square feet minimum | | |
| Setbacks (General) | All of the required setback area shall be landscaped and maintained unless otherwise allowed. | | |
| Setbacks Front | 15 feet minimum. | | |
| Setbacks Side | 5 feet min from property line. | 10 feet minimum from property line. | |
| Setback Rear | <ul style="list-style-type: none"> a) Initial Construction: 20 feet minimum from rear property line. b) Room Additions, Accessory Structures, and Enclosed Patios: Minimum setback shall be 10 feet from rear property line. c) Patio Covers: Minimum setback shall be 5 feet from the rear property line, as measured from the overhang of the patio cover. Interior lot line. <ul style="list-style-type: none"> d) Buildings containing living units and accessory structures not specified in subdivision (b) of this subsection: 15 feet. e) Garages and carports: zero setback may be approved if: <ul style="list-style-type: none"> i. The zero setback does not abut single family residential properties ii. Garages and carports match the color, materials, and roof slope of the main structures in the project | | |
| Allowable Setback Projections | <ul style="list-style-type: none"> a) Ground Floor. <ul style="list-style-type: none"> i. Awnings and canopies over windows: 60 inches maximum. ii. Barriers for defining outdoor dining areas such as fences, railings, planter boxes: as needed to encompass outdoor dining area. iii. Sun Shade Structures: 15 feet maximum. iv. Bay Windows: 3 feet maximum. v. Cornices, belt courses, and similar architectural features: 12 inches maximum. vi. Eaves, roof overhangs: 30 inches maximum. vii. Uncovered porches, decks, and landings (may be covered by arbors or trellises): 10 feet maximum. b) Above Ground Floor. Awnings, galleries, balconies, bay windows: 48 inches maximum. c) For Signs, see Section 4.5. All projections must maintain a minimum of 8 feet vertical clearance from ground. | | |
| Public Right-of- Way Encroachments (all require | <ul style="list-style-type: none"> a) Art, as determined by the approval of the public art component of the project. b) For signs, see Signs – Advertising Structures, 23.90 of the PMC. | | |

| Table 4.2: Multi-Family Flex Development Standards | | | |
|---|--|--|-----------|
| Subareas | GC | GN | GE |
| approval of an encroachment permit) | | | |
| Building Separation | 15 feet min. | | |
| Building Orientation | Primary ground-floor common entries or individual exterior dwelling unit entries shall be oriented to Chapman Corridor. | Primary ground-floor common entries or individual exterior dwelling unit entries shall be oriented to Chapman Corridor. However, with multiple structures, may orient buildings so that only the primary driveway or private road into the project is accessed from Chapman Corridor. Then buildings should be oriented to front a common building (such as a club house) or open space amenity accessed by the entry driveway. The backside of any building shall not face Chapman Avenue. Entrances at building corners may be used to satisfy this requirement. | |
| Site Design | <ul style="list-style-type: none"> a) New development projects are required to provide convenient pedestrian access to and from public sidewalks from either individual units or a common pedestrian walkway. b) If adjacent to a single-family zone, the third floor shall be stepped back a minimum of 10 feet. c) Garages and parking areas shall not front onto Chapman Avenue. d) Street frontage devoted to driveways and vehicular access shall be minimized to the greatest extent possible. e) Consolidated driveways are encouraged along Chapman Avenue. | | |
| Open Space Common and Private Minimum Requirements | <ul style="list-style-type: none"> a) 200 square feet per dwelling unit (total common and private) b) Minimum Dimensions: <ul style="list-style-type: none"> i. Private areas: 7 feet in any direction ii. Common areas: 20 feet in any direction | | |
| Types of Common Open Space Permitted | <p>A minimum of one shared outdoor recreational facility shall be required per project.</p> <p>Qualifying shared common open spaces: Park, clubhouse, rooftop terrace, swimming pool, gymnasium, tennis court, pickleball court, basketball court (half or full), children’s play area, community garden, courtyard, fire pit, dog park, barbeque area.</p> | | |
| <i>NOTE: If the Standards of the MFF zone are silent on a particular topic, the R-3 standards of the PMC shall apply.</i> | | | |

4.3 Non-residential Development Standards

4.3.1 Commercial Corridor

The commercial corridor designation applies to properties at the far end of the Gateway West subarea. Properties in this designation shall comply with the standards in Table 4.3.

| Table 4.3: Commercial Corridor Development Standards | |
|---|---|
| Land Use Designation | Commercial Corridor (CC) |
| Subarea | GW |
| Intensity (Floor Area Ratio) | 1.0 |
| Height | 35 feet max. |
| Ground Floor Height | 15 feet floor to ceiling height. |
| Setbacks (General) | All of the required setback area shall be landscaped and maintained unless otherwise allowed. |
| Setbacks Front | 15 feet min. |
| Setbacks Side and Rear | Adjacent to residential: 10 feet min. Adjacent to public right-of-way: 15 feet min. Side and rear yards are required only along a property lines adjacent to a residential district or a public right-of-way. |
| Allowable Setback Projections | <ul style="list-style-type: none"> a) Ground Floor. <ul style="list-style-type: none"> i. Awnings and canopies over windows: 60 inches maximum. ii. Barriers for defining outdoor dining areas such as fences, railings, planter boxes: as needed to encompass outdoor dining area. iii. Sun Shade Structures: 15 feet maximum . iv. Bay Windows: 3 feet v. Cornices, belt courses, and similar architectural features: 12 inches maximum. vi. Eaves, roof overhangs: 30 inches. vii. Uncovered porches, decks and landings (may be covered by arbors or trellises): 10 feet maximum. b) Above Ground Floor. Awnings, galleries, balconies, bay windows: 48 inches maximum. c) For signs, see standards in Section 4.5. <p>All projections must maintain a minimum of 8 feet vertical clearance from ground.</p> |
| Public Right-of- Way Encroachments | <ul style="list-style-type: none"> a) Art, as determined by the approval of the public art component of the project. b) For signs, see standards in Section 4.5. c) Fences, only allowed in conjunction with a restaurant use. <p>NOTE: Requires approval of an encroachment permit.</p> |
| Frontage Requirements | <p>A section of blank wall shall not exceed 20 linear feet without being interrupted by a window or entry or other façade treatment.</p> <p>Developments over 1 acre in size shall choose a frontage type from the Frontage</p> |

| Land Use Designation | Commercial Corridor (CC) |
|-----------------------------|--|
| Subarea | GW |
| | Section of Chapter 5, Design Standards and Guidelines. |
| Building Orientation | Buildings shall be oriented to Chapman Avenue. Pedestrian connections shall be provided to Chapman Avenue. |
| Outdoor Dining | <ul style="list-style-type: none"> a) All outdoor dining areas shall maintain a minimum sidewalk clearance of 4 feet between any outdoor eating area and light poles, signposts, utility poles, and similar sidewalk obstructions. b) Alcoholic beverages may be served in an outdoor eating area of an eating establishment provided the business operating the outdoor eating area has obtained an approval authorized by this Plan and the PMC to serve alcoholic beverages on-site and has obtained and maintains all approvals required by the ABC. c) Outdoor dining areas within the public right-of-way are subject to an Encroachment Permit. d) See Chapter 5 Section 5.15 Outdoor Dining of this Plan for design related standards. |

4.4 Mixed-Use Development Standards

4.4.1 Mixed-Use and Mixed-Use Civic Center

The Mixed-Use and Mixed-Use Civic Center designations occur in three subareas described below. The mixed-use designations implement the City's General Plan Mixed-Use designation and allow for vertical and horizontal mixed-use. The standards in Table 4.4 shall apply for the identified subareas.

Gateway West (GW). Mixed-use properties in the Gateway West subarea adjacent to the Commercial Corridor designation. Mixed-use options in this area include hotel and retail and/or residential land uses. The properties are ready for redevelopment with many underutilized parcels in this subarea.

Gateway Core (GC). Properties in the Gateway Core subarea are adjacent to both Old Town and single-family development. New development should provide a transition between buildings that vary in size and height, preserving privacy for all residents. Lot consolidation in this subarea is encouraged to provide projects at a size and scale that makes development feasible.

Gateway East (GE). Properties in the Gateway Neighborhood subarea include the properties at three of the four corners of the Chapman Avenue and Kraemer Avenue intersection, including the Civic Center. This subarea includes the Kraemer Land property that is expected to be a catalyst site for the CCRP area.

| Land Use Designation | Mixed-Use | | | Mixed-Use Civic Center |
|-------------------------------|-----------|-----------|-----------|------------------------|
| | GW | GC | GE | GE |
| Density/ Intensity Max | DU/AC: 45 | DU/AC: 35 | DU/AC: 35 | DU/AC: 45 |

Table 4.4: Mixed-Use Development Standards

| Land Use Designation | Mixed-Use | | | Mixed-Use Civic Center |
|---|---|---|---|--|
| | Subareas | GW | GC | GE |
| Residential (du/ac) Non-residential (Floor Area Ratio FAR) | FAR: 1.0 | FAR: 0.5 | FAR: 0.5 The catalyst site shall be a minimum of one acre and shall contain no less than 65 dwelling units per acre. | FAR: 0.35 |
| Setbacks (General) | All of the required setback area shall be landscaped and maintained unless otherwise allowed. | | | |
| Setbacks Front | 15 feet min. | 10 feet min. For corner lots, a 15 foot minimum setback along both street frontages. | | |
| Setbacks Side and Rear | Side and rear yards are required only along a property line adjacent to a residential district or a public right-of-way. The required yard shall be a minimum of 15 feet adjacent to a residential district and 10 feet adjacent to a public right-of-way. All of the required yard area shall be landscaped and maintained as a condition of the use. | | | |
| Allowable Setback Projections. | <ul style="list-style-type: none"> a) Ground Floor. <ul style="list-style-type: none"> i. Awnings and canopies over windows: 60 inches maximum. ii. Barriers for defining outdoor dining areas such as fences, railings, planter boxes: as needed to encompass outdoor dining area. iii. Sun Shade Structures: 15 feet maximum. iv. Arcade: 15 feet maximum. v. Cornices, belt courses, and similar architectural features: 12 inches maximum. vi. Eaves, roof overhangs: 30 inches maximum. vii. Uncovered porches, decks and landings (may be covered by arbors or trellises): 10 feet maximum. b) Above Ground Floor. Awnings, galleries, balconies, bay windows: 48 inches maximum. c) For signs, see Section 4.5. d) All projections must maintain a minimum of 8 feet vertical clearance from ground. | | | |
| Public Right-of- Way Encroachments | <ul style="list-style-type: none"> a. Art, as determined by the approval of the public art component of the project. b. Signs, see Section 4.5 below. c. Outdoor Dining. d. Fences, only allowed in conjunction with a restaurant use. <p><i>NOTE: Require approval of an encroachment permit.</i></p> | | | |
| Building Height | 3 to 4 stories—55 feet-65 feet max—depends on adjacency 30 feet max. If adjacent to SFF or R-1, 35-foot maximum height shall be permitted when 2 feet of additional setback is provided for every foot of additional height. Catalyst Site: 5 stories/ 60 feet max. The catalyst site may exceed maximum allowed building height by one floor. | | | 40 feet max. Rear building with stepback; 30 feet max adjacent to single-family homes. |
| Ground Floor Height | Where commercial uses are on ground floors, those commercial uses shall have a minimum 15-foot height floor to ceiling. | | | |

Table 4.4: Mixed-Use Development Standards

| Land Use Designation | Mixed-Use | | | Mixed-Use Civic Center |
|---|---|----|----|------------------------|
| | Subareas | GW | GC | GE |
| Frontage Requirements | <p>A section of blank wall shall not exceed 20 linear feet without being interrupted by a window or entry or other façade treatment.</p> <p>Developments over 1 acre in size shall choose a frontage type from the Chapter 5 Design Guidelines, Section 5.7 Frontages, of this Plan.</p> | | | |
| Building Orientation | <p>Primary ground-floor common entries or individual exterior dwelling unit entries shall be oriented to Chapman Corridor. Development with multiple structures may orient buildings so that only the primary driveway or private road into the project is accessed from Chapman Corridor. Then buildings should be oriented to front a common building (such as a club house or building containing the hotel lobby) or open space amenity accessed by the entry driveway. The backside of any building shall not face Chapman Avenue. Entrances at building corners may be used to satisfy this requirement.</p> <p>Pedestrian connections shall be provided to Chapman Avenue.</p> | | | |
| Open Space Common and Private Minimum Requirements | <p>a) 200 square feet per dwelling unit (total common and private).</p> <p>b) Minimum Dimensions.</p> <p>i. Private areas: 7 feet in any direction.</p> <p>ii. Common areas: 20 feet in any direction.</p> <p>Open space and outdoor open space areas may be provided as private areas, common areas, or a combination.</p> <p>All common open space areas shall be conveniently located and readily accessible from all dwelling units on the building site and shall be contiguous to and integrated with other common areas on the building site.</p> <p>The common open space areas may be composed of active or passive facilities and may incorporate any required setback areas other than street setback areas and required landscape setbacks, but shall not include or incorporate any driveways or parking areas, trash pickup or storage areas, or utility areas. All residential common areas shall be improved as either active or passive facilities, with landscape or hardscape elements designed to serve the residents of the project.</p> | | | |
| Types of Common Open Space Permitted | <p>A minimum of one shared outdoor recreational facility shall be required per project.</p> <p>Qualifying shared common open spaces: Park, clubhouse, rooftop terrace, swimming pool, gymnasium, tennis court, pickleball court, basketball court (half or full), children’s play area, community garden, courtyard, fire pit, dog park, barbeque area.</p> | | | |
| Outdoor Dining | <p>a) All outdoor dining areas shall maintain a minimum sidewalk clearance of 4 feet between any outdoor eating area and light poles, signposts, utility poles, and similar sidewalk obstructions.</p> <p>b) Alcoholic beverages may be served in an outdoor eating area of an eating establishment provided the business operating the outdoor eating area has obtained an approval authorized by this Plan and the PMC to serve alcoholic beverages on-site and has obtained and maintains all approvals required by the ABC.</p> <p>c) Outdoor dining areas within the public right-of-way are subject to an Encroachment Permit.</p> <p>d) See Chapter 5 Section 5.15 Outdoor Dining of this Plan for design related standards.</p> | | | |

4.5 Sign Regulations

4.5.1 Purpose and Intent

The sign regulations for CCRP are intended to appropriately address the placement, type, size, and number of signs allowed in Chapman Corridor and to require the proper maintenance of signs.

The purpose of these requirements are to:

- 1) Avoid traffic safety hazards for motorists, bicyclists, and pedestrians caused by visual distractions and obstructions.
- 2) Promote the aesthetic and environmental values of the community by providing for signs that do not impair the attractiveness of the City as a place to live, work, and shop.
- 3) Provide for signs as an effective channel of communication while ensuring that signs are aesthetically proportioned in relation to adjacent structures and the structures to which they are attached.
- 4) Safeguard and protect the public health, safety, and general welfare.
- 5) Promote the pedestrian scale of the corridor.

4.5.2 Applicability

- 1) These sign regulations apply to all signs in the CCRP area except: directional/instructional signs and real estate signs shall instead comply with the requirements of the City's Zoning Code (PMC 23.90); and specialty City identification/ gateway signage.
- 2) The provisions of this section do not regulate the message content of a sign (sign copy) regardless of whether the message content is commercial or noncommercial.
- 3) Sign installation in the CCRP shall require sign permit approval in compliance with the City's Zoning Code (PMC 23.90), unless exempted from sign permit requirements.
- 4) Sign Variances: See the Zoning Code (PMC 23.90.220).
- 5) Definitions of the specialized terms and phrases used in this section are in the Zoning Code (PMC 23.90).

4.5.3 Prohibited Signs

- 1) Abandoned signs (includes signs on abandoned or closed businesses).
- 2) Animated and moving signs, including electronic message display signs; and variable intensity, blinking, or flashing signs; or signs that emit a varying intensity of light or color, except time and temperature displays (which are not considered signs).
- 3) Exposed cabinet/raceways behind channel letters.
- 4) Internally illuminated cabinet (can) signs.

- 5) Off-site signs (e.g., billboards, signs mounted on vehicles).
- 6) Obscene signs.
- 7) Pole signs and other freestanding signs over six feet in height.
- 8) Roof signs.
- 9) Signs that simulate, in color, size, or design, any traffic control sign or signal, or that make use of words, symbols, or characters in a manner that interferes with, misleads, or confuses pedestrian or vehicular traffic.
- 10) A sign burned, cut, or otherwise marked on or affixed to a rock, tree, or other natural feature.
- 11) A sign placed within a public right-of-way, except as provided by Tables 4.5 through 4.7.
- 12) A sign painted directly on a building, unless approved as a Creative Sign (See Section 4.5.9 Creative Sign Permit, of this Plan) .
- 13) Permanent signs that advertise continuous sales, special prices, or include phone numbers.
- 14) Temporary signs, including:
 - a. Balloons and other inflatable devices.
 - b. Flags, except official national, state, or local government, institutional, or corporate flags properly displayed.
 - c. Pennants and streamers, except in conjunction with an athletic event, carnival, circus, or fair.

4.5.4 General Requirements for All Signs

- 1) Sign Area and Height Measurement. The measurements of sign area and height shall comply with the City's Zoning Code (PMC 23.90).
- 2) Sign Location Requirements. Each sign shall be located according to the following requirements and all other applicable provisions of this chapter.
- 3) On-Premises Signs Required. Each sign shall be located on the same site as the subject of the sign, except as otherwise allowed by this chapter.
 - a. Setback Requirements. Each sign shall comply with the setback requirements of the applicable zoning district, except for an approved projecting sign, and except for an approved freestanding sign, which shall be set back a minimum of 5 feet from the front and side street property lines.
 - b. Placement on a Building. No sign shall be placed so as to interfere with the operation of a door or window. Signs should not be located so that they cover prominent architectural features of the building.
 - c. Signs within a Public Right-of-Way. No sign shall be allowed in the public right-of-way except for the following:
 - i. A projecting sign in compliance with Tables 4.5 through 4.7.
 - ii. Public signs erected by or on behalf of a governmental agency to convey public information, identify public property, post legal notices, or direct or regulate pedestrian or vehicular traffic.

- iii. Bus stop signs installed by a public transit company.
 - iv. Informational signs of a public utility regarding its lines, pipes, poles, or other facilities.
 - v. Emergency warning signs erected by a governmental agency, a public utility company, or a contractor doing authorized work within the public right-of-way.
 - d. Any sign installed or placed within the public right-of-way other than in compliance with this section shall be forfeited to the public and be subject to confiscation.
- 4) Sign Design. The following design criteria shall be used in reviewing the design of individual signs. Substantial conformance with each of the following design criteria shall be required before a sign permit or building permit can be approved.
- 5) Color. Colors on signs and structural members should be harmonious with one another and relate to the dominant colors of the buildings on the site. Contrasting colors may be utilized if the overall effect of the sign is still compatible with building colors.
- 6) Design and Construction.
- a. Except where otherwise permitted elsewhere in this section, each sign shall be constructed of permanent materials and shall be permanently attached to the ground, a building, or another structure by direct attachment to a rigid wall, frame, or structure.
 - b. Each permanent sign shall be designed by a professional (e.g., architect, building designer, landscape architect, interior designer, or others whose principal business is the design, manufacture, or sale of signs), or who is capable of producing professional results.
 - c. Each permanent sign shall be constructed by persons whose principal business is building construction or a related trade, including sign manufacturing and installation, or others capable of producing professional results. The intent is to ensure public safety, achieve signs of careful construction, neat and readable copy, and durability to reduce maintenance costs and prevent dilapidation.

- 7) Materials and Structure.
- a. Sign materials (including framing and supports) shall be representative of the type and scale of materials used on the site where the sign is located. Sign materials shall be complementary those used on the buildings on the site and any other signs on the site.
 - b. No sign shall include reflective material.
 - c. Materials for permanent signs shall be durable and capable of withstanding weathering over the life of the sign with reasonable maintenance.
 - d. The size of the structural members (e.g., columns, crossbeams, and braces) shall be proportional to the sign panel they are supporting.
 - e. The use of individual letters incorporated into the building design is encouraged, rather than a sign with background and framing other than the structure wall.
 - f. See Design Guidelines, Chapter 5, for additional guidelines for materials.
- 8) Street Address. The review authority may require that a sign include the street address of the site where it determines that public safety and emergency vehicle response would be more effectively served than if the street address were displayed on 1 or more buildings on the site.
- 9) Copy Design Guidelines. The city does not regulate the message content (copy) of signs; however, the following are principles of copy design and layout that can enhance the readability and attractiveness of signs. Copy design and layout consistent with these principles is encouraged but not required.
- a. Sign copy should relate only to the name and/or nature of the business or commercial center.
 - b. Permanent signs that advertise continuous sales, special prices, or include phone numbers are prohibited.
 - c. Information should be conveyed briefly or by logo, symbol, or other graphic manner. The intent should be to increase the readability of the sign and thereby enhance the identity of the business.
 - d. The area of letters or symbols should not exceed 40 percent of the sign area in commercial uses or 60 percent for residential uses.
 - e. Freestanding signs should contain the street address of the parcel or the range of addresses for a multi-tenant center.

4.5.5 Sign Lighting

Sign lighting shall be designed to minimize light and glare on surrounding rights-of-way and properties.

- 1) External light sources shall be directed and shielded so that they do not produce glare off the site, or on any object other than the sign.
- 2) Sign lighting shall not blink, flash, flutter, or change light intensity, brightness, or color.
- 3) Colored lights shall not be used at a location or in a manner so as to be confused with or construed as traffic control devices.
- 4) Neither the direct nor reflected light from primary light sources shall create hazards for pedestrians or operators of motor vehicles.

- 5) For energy conservation, light sources shall be hard-wired fluorescent or compact fluorescent lamps, or other lighting technology that is of equal or greater energy efficiency. Incandescent lamps are prohibited.

4.5.6 Sign Maintenance

- 1) Each sign and supporting hardware, including temporary signs and awning signs, shall be maintained in good repair and functioning properly at all times. Any damage to a sign or its illumination, including the failure of illumination, shall be repaired within a maximum of 14 days from the date of damage or failure.
- 2) A repair to a sign shall be of materials and design of equal or better quality as the original sign.
- 3) A sign that is not properly maintained and is dilapidated shall be deemed a public nuisance and may be abated in compliance with the City's Zoning Code.
- 4) When an existing sign is removed or replaced, all brackets, poles, and other supports that are no longer required shall be removed, and any/all damage to the exterior of the building shall be repaired/repainted to the satisfaction of the development services director or designee. When an existing sign is removed, the repairs must match surface type and color of building exactly. No disparity in color tone is permitted.

4.5.7 Sign Standards by Use

Each sign shall comply with the standards provided by this section and comply with the requirements in Tables 4.5 through 4.7, except as permitted by the approval of a creative sign permit, described below.

| Allowed Sign | Maximum Sign Height | Maximum No. of Signs Allowed per Parcel | Maximum Sign Area Allowed per Parcel |
|----------------------|---|--|---|
| Wall or Freestanding | Wall signs: below edge of roof. Placement shall only be located below the roof or over the primary entrance. Freestanding: 48 inches | 1 wall sign or freestanding sign per primary vehicular entrance or street frontage | 12 square feet per face area; 24 square feet maximum total square feet for all signs. |

| Allowed Sign | Placement Standards | Maximum Number and Sign Area |
|--|---|--|
| Awning | Shall be entirely on awning valence; lettering max 66% of valence height; valence height max: 18 inches. | 50% of the area of the valence front. 1 sign max per separate awning valence. |
| Marquee | To be established during site plan review. Minimum clearance of 10' from sidewalk surface below. Allowed only for the entrance of a theatre or playhouse. | To be established during site plan review. 3 square feet per 1 linear foot of frontage width, up to 80 percent of frontage width. 1 sign max. |
| Monument | 5 feet including base structure. Allowed only on a site with more than 100 feet of continuous street frontage. | 36 square feet. |
| Projecting or Suspended | Up to 24 inches from face of building and bottom of sign shall be no closer than 8 feet above sidewalk surface below. | 6 square feet. No dimension greater than 3 feet. |
| Wall | 2 feet below parapet or eave. Individual letters 18 inches. Mounting 1-story: above 1st-floor windows. Mounting multi-story: between windows. | 1 square foot per linear foot primary business. 1 sign allowed per business frontage with pedestrian entrance. Side street or rear entrance wall sign max 50% of the primary sign area. |
| Window Permanent | Within window area. | 15% of total window area max. |
| Window Temporary | Within window area. | 25% of total window area. Allowed for display a maximum of 15 days at 1 time, up to 3 times in a 12-month period. |
| A-Boards and Other Portable Sidewalk Signs | May not impede pedestrian flow. | 1 per business. Signs are only be permitted while the business is open. Requires an encroachment permit if in the public right-of-way. |
| Directional Signage on Private Property | Cannot be freestanding and shall be minimized for basic directional functions. Sign may include minimized development logo and/or branding associated with development or tenant. | Included in sign permit application approval process. |

| Allowed Sign | Placement Standards | Maximum Number and Sign Area |
|--------------|---|---|
| Monument | 5 feet max height, including base structure. | 1 per site. 25 square feet sign area max. |
| Wall signs | Raised letter only, unlighted or dimly back-lighted | Not to exceed one-half square foot in area per foot of front of building, or 50 square feet total area per building, whichever is less. |

Banners, pennants, and streamers are specifically prohibited.

4.5.8 Master Sign Program

All mixed-use projects shall require a master sign program, which is reviewed and approved by the decision-making body in each case. A “master sign plan” means a coordinated program of signage for new or existing commercial, office, or residential that has more than one business establishment or tenant. The master sign program can permit signs that meet the intent and standards of the Sign Code and ensure that all signs are integrated thoughtfully into the design of the structures, creating a unified architectural statement. The master sign program provides a means for defining common sign regulations for multi-tenant projects to encourage maximum incentive and latitude in the design and display of multiple signs, and to achieve, not circumvent, the intent of this chapter.

- 1) **Application Requirements and Revisions to Master Sign Programs.** A sign permit application for a master sign program shall include all information and materials required by the development services department and the filing fee set by the City’s fee resolution. Revisions to a master sign program may be approved by the director of development services with a standard sign permit if the intent of the original approval is not affected. Revisions that would substantially deviate from the original approval shall require the approval of a new master sign program.
- 2) **Standards.** A master sign program shall comply with the following standards:
 - a. The program shall comply with the purpose of this chapter.
 - b. The signs shall enhance the overall development, be in harmony with, and relate visually to other signs in the master sign program, to the structures or developments they identify, and to surrounding development;
 - c. The program shall accommodate future revisions that may be required because of changes in use or tenants; and
 - d. The program shall comply with the standards of this chapter, except that flexibility is allowed with regard to sign area, number, location, or height to the extent that the master sign program will enhance the overall development and will more fully accomplish the purposes of this chapter.
 - e. Sign permit application must include identification of size, height, and location of all signs.

4.5.9 Creative Sign Permit

- 1) **Definition of Creative Sign Permit.** Applicants may apply for a creative sign permit for signs that are not listed or that exceed the provisions of this chapter. The creative sign permit is intended for signs that meet the intent and standards of the sign code, but may not necessarily meet the standards shown in

Tables 4.5 through 4.7 of this chapter. An applicant may request approval of a creative sign permit to authorize on-site signs that use different standards from the other provisions of this chapter but comply with the intent of this chapter.

- 2) **Revisions** to a master sign program may be approved by the director with a standard sign permit if the intent of the original approval is not affected. Revisions that would substantially deviate from the original approval shall require the approval of a new master sign program.
- 3) **Purpose.** To encourage signs of unique design and that exhibit a high degree of thoughtfulness, branding, imagination, inventiveness, and spirit; and to provide a process for the application of sign regulations in ways that will allow creatively designed signs that make a positive visual contribution to the overall image of the city, while mitigating the impacts of large or unusually designed signs.
- 4) **Application and Procedure Requirements.** A sign permit application for a creative sign shall include all information and materials required by the department, and the filing fee set by the City's resolution fee. A sign permit application for a creative sign shall be subject to review and approval by the director of development services when the proposed sign is 50 square feet or less, and by the planning commission when the sign is larger than 50 square feet. Notification for a sign permit for a creative sign shall be given in the same manner specified by the zoning ordinance for director-approved development permits.
- 5) **Design Criteria.** In approving an application for a creative sign, the review authority shall ensure that a proposed sign meets the following design criteria:
 - a. Constitutes a substantial aesthetic improvement to the site and has a positive visual impact on the surrounding area.
 - b. Is of unique design and exhibits a high degree of thoughtfulness, imagination, inventiveness, and spirit.
 - c. Provides strong graphic character through the imaginative use of graphics, color, texture, quality materials, scale, and proportion.
- 6) **Contextual Criteria.** The sign shall contain at least one of the following elements:
 - a. Classic historic design style.
 - b. Creative image reflecting current or historic character of the city.
 - c. Symbols or imagery relating to the citrus packing industry.
 - d. Inventive representation of the use, name, or logo of the structure or business.
- 7) **Architectural Criteria.** The sign shall:
 - a. Utilize or enhance the architectural elements of the building.
 - b. Be placed in a logical location in relation to the overall composition of the building's façade and not cover any key architectural features and details of the façade.
- 8) **Neighborhood Impacts.** The sign shall be located and designed not to cause light and glare impacts on neighboring residential uses.

4.5.10 Legal Nonconforming Signs

A legal nonconforming sign is any permanent or temporary sign that was legally established and maintained in compliance with the provisions of all applicable laws in effect at the time of original installation but that does not now comply with the provisions of this specific plan.

- 1) **General Requirements.** A legal nonconforming sign shall not be:
 - a. Changed to another nonconforming sign.
 - b. Structurally altered to extend its useful life.
 - c. Enlarged.
 - d. Re-established after a business is discontinued for 60 days or more, subject to the amortization clause in Section 4.5.6.
 - e. Re-established after damage or destruction to 50 percent or more of the value of the sign or its components, as determined by the building official and subject to the amortization clause below.
- 2) **Maintenance and Changes.** Sign copy and face changes, nonstructural modifications, and nonstructural maintenance (e.g., painting, rust removal) are allowed without a sign permit up to a maximum of 25 percent of the existing total area of the sign. Face changes not including copy, and any nonstructural modifications exceeding 25 percent of the existing total area of the sign, and any structural changes shall comply with all applicable standards of this chapter.

4.5.11 Amortization and existing uses.

- 1) In order to preserve private property rights, all legal uses, buildings or structures in existence immediately preceding the effective date of the ordinance codified in this chapter, may be continued to operate as a legal nonconforming use, building or structure pursuant to PMC Ch. 23.84.

4.5.12 Fences and Walls

Must comply with Chapter 23.81 of the Placentia Municipal Code.

4.5.13 Trash and Recycling Enclosures

Must comply with Chapter 23.41 of the Placentia Municipal Code.

4.5.14 Parking

Parking for all uses must comply with Chapter 23.78 of the Placentia Municipal Code. Additionally, mixed-use projects shall provide a parking management plan for approval by the Development Services Director or designee. The PMP shall detail joint, or reciprocal parking between uses or buildings.

DRAFT

5 DESIGN STANDARDS AND GUIDELINES

5.1 Introduction

The design standards and guidelines are intended to promote quality design, consistent with overall vision, while providing a level of flexibility to encourage creative design. This chapter provides additional standards and guidelines that address the physical design of buildings and various site design elements within the Chapman Corridor Revitalization Plan (CCRP) area in addition to those found in Chapter 4. These standards apply to all subareas of the corridor.

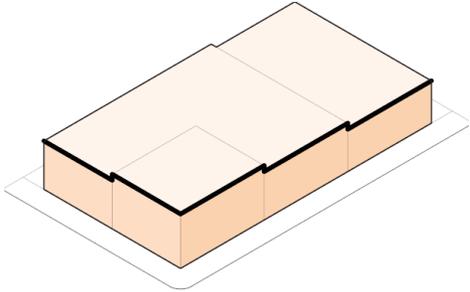
5.1.1 Interpretation

This chapter is intended to provide guidance and expectations for quality building and site design for development along Chapman Corridor. Design standards are provided with specific requirements and are written as “shall” or “must.” A design guideline written as a “should” requires compliance unless a legitimate reason or acceptable design substitute is deemed acceptable through the Development Plan Review process, as described in Chapter 7 of this Plan. A design guideline written with an action verb (e.g., provide, use, locate, create, establish, employ) is highly recommended. Finally, statements written as “prohibited” or “not allowed” is not appropriate for the CCRP area.

5.2 Building Massing and Scale

- 1) Projects containing several buildings should be designed as a collection of suitably scaled buildings instead of a singular mass.
- 2) Buildings greater than three stories should provide variation by using balconies, fenestration, and sunshades to create an interesting pattern of projections and recesses.
- 3) Building mass should be articulated to reflect a human scale, both horizontally and vertically. Examples of such building elements include articulated façades, corner elements, inset windows, highlighted entry features, and prominent cornices and rooflines. See sections below for additional requirements and guidelines for these elements.

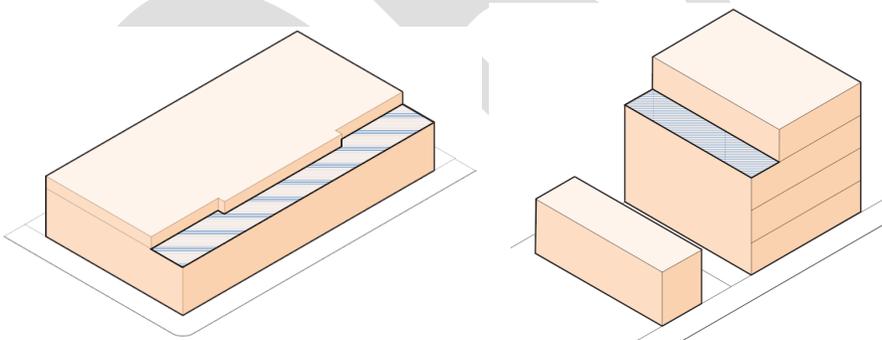
5.3 Building Height



Modulation of rooflines can add visual interest to the skyline, while reducing the scale of large building masses. Modulation can also minimize the “canyon” effect along a corridor while creating visual reference points and the appearance of distinct and separate buildings.

- 1) Development shall not exceed the height limits set in Chapter 4.
- 2) Buildings over three stories in height that do not use stepbacks (sec. 5.2.4) or prominent corners (sec. 5.2.6) shall vary the roof height by at least 5 feet for every 75 linear feet.
- 3) Rooftop equipment shall be concealed from visibility from the public right-of-way. The point of view for determining visibility shall be 6 feet above grade at a distance of 200 feet. If the roof structure does not provide this screening, an equipment screen shall be included in the design.
- 4) Exception(s): Projecting features such as appurtenances, chimneys, and vents may exceed the height limit.

5.4 Stepbacks

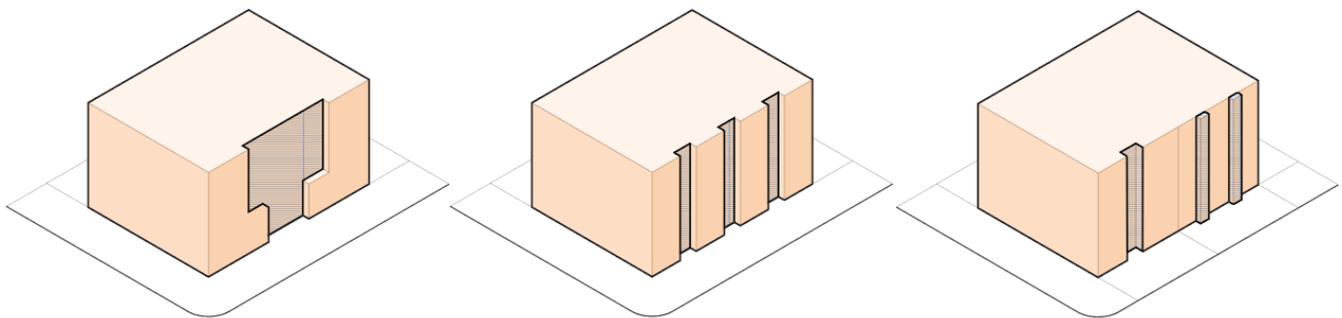


Stepbacks are upper levels of a building set back from the façade before the total height of the building is achieved. Building stepbacks at upper levels allow for multiple roofline profiles, lessening the imposing appearance of building massing, providing privacy, and creating space for open space amenities.

- 1) Chapter 4 establishes when and where (which building façade) stepbacks are required.
- 2) Stepbacks are measured from the primary façade plane. The primary façade plane is defined as the vertical plane with the greatest surface area above the ground floor.

- 3) Allowable Uses. Uses allowed within the stepback include:
 - Private and outdoor common open spaces
 - Shade structures, such as trellises, or fabric canopies
 - Green roofs and photovoltaic panels
- 4) Exception(s): Architectural features used to meet prominent corners are allowed to interrupt required stepbacks.

5.5 Façade Articulation and Design

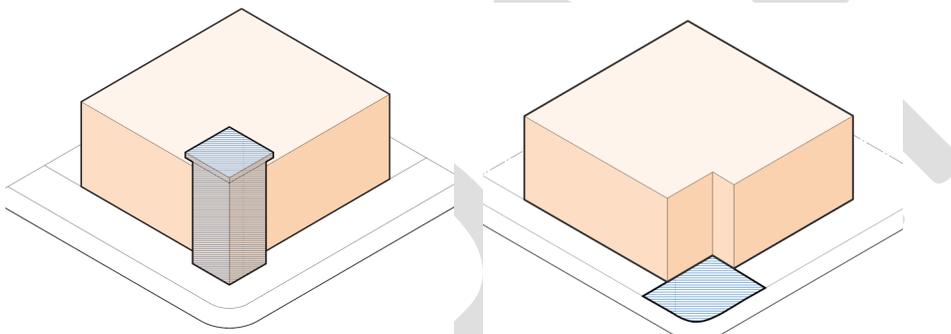


Façade articulation encourages proportions that reduce the apparent massing of buildings and create visual interest. Recesses and projections of a building facade, in combination with detailing, articulation, and fenestration pattern, create diversity and visual interest in the horizontal, particularly in its relationship to the pedestrian realm.

- 1) Façade Articulation. Any street-facing façade exceeding 50 feet in length shall modulate a minimum of 25 percent of the area above the first story. This modulation shall be between 2 feet and 12 feet in depth from the primary façade plane. Buildings 2 stories or less are exempt.
- 2) Modulation is not required to be continuous or open to the sky, and may be recessed or projected.
- 3) Building façades should include three-dimensional detailing such as cornices, belt courses, window moldings, bay windows, and reveals to create shadows and façade relief.
- 4) Balconies that are recessed a minimum of 2 feet shall qualify. Projected balconies shall not qualify.
- 5) Façade Articulation, Large Buildings. Any street-facing façade exceeding 150 feet shall additionally include a minimum break of 10 percent of the façade length or 20 feet, whichever is greater. This break shall be a minimum of 8 feet deep and open to the sky.
- 6) Materials, texture, patterns, colors, and details on building façades should vary to diminish the perceived mass of large buildings and to create the impression of smaller-scale buildings.
- 7) Building façades should be articulated with a building base, body, and roof or parapet edge.

- 8) The highest level of details should be on the ground floor's front façade and façades visible from public streets. However, similar and complementary massing, materials, and details should be incorporated into side and rear façades.
- 9) No more than 40 percent of balconies on a project shall extend beyond the façade of the building, and the maximum projection shall be 25 percent of the balcony's full depth. All other balconies shall be recessed into the building façade.
- 10) No balconies shall overhang into the public right-of-way or cross a property line.

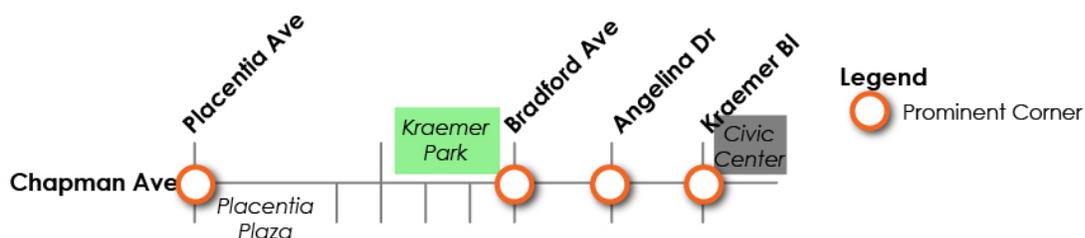
5.6 Prominent Corners



Sites at prominent street intersections present unique opportunities to enhance the aesthetic quality of gateways into Chapman Corridor by creating signature buildings, architectural features, or open spaces. In addition to creating variation in building massing, an enhanced corner can create a sense of place in the public realm by incorporating key design features.

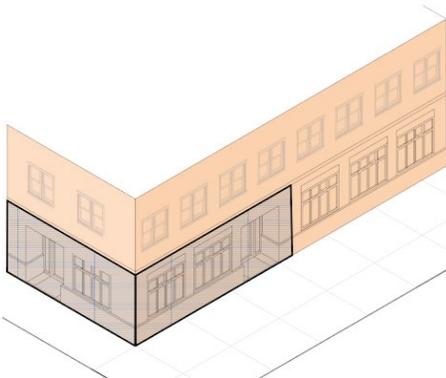
- 1) Location. Chapman Avenue at Placentia Avenue, Bradford Avenue, Angelina Drive, and Kraemer Boulevard.
(See Figure 5-1: Prominent Corners)

Figure 5-1: Prominent Corners



- 2) Design. Corner features shall have a distinct but complementary design to the overall mass of the building.
- 3) Minimum Height. If expressed as architectural form, corner features shall be greater in height than the overall building mass by a minimum of 10 feet, and may exceed the maximum allowable height established in Chapter 4 by 10 feet.
- 4) Canopies / Marquees. Enhanced canopies or marquees shall be provided to help create a sense of enclosure in the public realm.
- 5) Open Space. Open space in the form of plazas or entry courts may be used as an alternative to architectural form to meet prominent corner requirements.
- 6) Stepback Exemptions. Sites designated “prominent corners” (see Figure 5-1: Prominent Corners) are exempt from any stepback requirements for up to 30 percent of the street-facing building facade at the corner.
- 7) Exception(s): Prominent corner requirements shall not apply to projects with assembled parcels under 0.5 acre in size.
- 8) Move Figure 5-1 Here.

5.7 Frontages



The following building frontages have been selected for the Chapman Corridor area:

Arcade. Arcades are facades with a ground-floor colonnade that supports the upper stories of the building or, for one-story buildings, the roof. Arcades contain ground-floor shopfronts, making them ideal for retail or restaurant use, because the arcade shelters pedestrians from the sun and rain. Arcades function as an extension of the public sidewalk.

Gallery. The main facade of the building is set back at or near the front lot line, and an at-grade, covered structure, typically articulated with colonnade or arches, covers an area in the right-of-way. This type may be one or multiple stories and is intended for restaurants and outdoor dining uses.

Shopfront. A shopfront is a frontage wherein the building façade and entrance are at sidewalk grade and close to the pedestrian zone. Shopfronts include large areas of transparent openings and doors and are commonly

equipped with cantilevered roofs or awnings. Shopfronts typically provide access directly from sidewalks and are oriented to display ground-level commercial uses. This frontage type is typically used for commercial use. This frontage type can be used in conjunction with a terrace and/or forecourt to create a more engaging street.

Forecourt. A forecourt is a frontage wherein a portion of the building façade is recessed from the primary building façade. The forecourt may be used as an entry court and open space for residential uses, or as additional shopping or seating areas for commercial uses. This frontage type is appropriate for either residential and/or commercial uses. A mixed-use building could use a forecourt as a residential entrance while commercial uses occupy street-adjacent building space. This type can be used in conjunction with shopfronts and stoops as a transition into residential frontage.

The following also guide the design of building frontages for the CCRP area:

- 1) Active uses should be focused at the sidewalk level, with the greatest concentration at the intersection of two streets.
- 2) Entries to stores and ground-floor commercial uses should be visually distinct from the rest of the store façade, with inventive use of scale, materials, glazing, projecting or recessed forms, architectural details, color, and/or awnings. These entries should have direct at-grade access from the sidewalk.
- 3) Individual storefronts should be clearly defined by architectural elements, such as piers or changes in plane and/or materials.
- 4) For non-residential uses, a minimum of 60 percent of the façade between 3 and 12 feet above the sidewalk should contain windows of clear or lightly tinted vision glass that allows views of indoor space. Heavier tinted or mirrored glass is not permitted.
- 5) Residential units must be designed to ensure the security of residents. Secured entrances and exits that are separate from the non-residential uses and are directly accessible to resident parking areas are required.

5.8 Awnings, Canopies, and Marquees

- 1) Awnings, canopies, and marquees enhance the pedestrian environment by providing visual interest and a human scale. Their use is encouraged, but care must be taken so they do not negatively impact the pedestrian zone.
- 2) Ground supports for encroachments are prohibited.
- 3) A continuous series of awnings, canopies, or other coverings is encouraged along retail frontages. Awnings and canopies should be designed to correspond to individual storefront structural bays and should convey the outline and proportion of storefront window openings.

5.9 Landscaping

- 1) Plant materials should utilize plants and trees identified in the City's Parkway Design Guide.

- 2) Provide a variety of landscaped spaces that accommodate different activities and needs, including buffering from sidewalks, at the perimeter of parking areas and seating and dining areas.
- 3) The palette of plant materials in landscape buffers should create an aesthetically pleasing space through a mix of colors, heights, and types of plants.

5.10 Circulation and Access

- 1) All buildings should be connected to the public sidewalk by a clearly delineated path or walkway.
- 2) Primary routes for pedestrian circulation should provide universal access for people of all ages, sizes, abilities, or disabilities wherever possible by minimizing the number of steps and elevation changes and providing ADA access to building entries.
- 3) Design cues should be provided along pedestrian connections to help demarcate the transition between public and private spaces. Where there is a pedestrian connection between public and private space, at least one of the following design cues should be used:
 - Change in paving color
 - Change in paving materials
 - Landscaping on one or both sides of a walkway

5.11 Materials and Finishes

- 1) Buildings shall use durable, high quality materials to develop long-lasting buildings that can be adaptively reused over time.
 - Brick, natural stone, precast concrete, and factory-finished metal panels (heavy gauge only, in corrugated or flat sections) are preferred.
 - Alternatives to stucco are preferred. When stucco is used it should be applied with a smooth finish. Stucco seams should be used to create visual interest for the building's façade and form.
 - The finish, texture, and color of materials should be compatible with the overall architectural theme.
- 2) Greater attention to detail and quality should be used at the lower levels of a building to contribute to an enhanced streetscape.
- 3) Encourage buildings to express a variety of architectural styles, but with full awareness of and respect for the height, mass, articulation, and materials of the high quality (desirable) older buildings that surround them.
- 4) Architectural style and use of quality materials shall be consistent throughout an entire mixed-use project; however, variations in materials and details may be used to differentiate between the residential and commercial portions of the project.

- 5) Construction details should be authentic and applied with consistency. Faux architecture that mimics a past era is strongly discouraged.
- 6) Materials and colors should be used to imply form changes, particularly for entrance lobbies, massing changes, and different uses or tenants.
- 7) Entrance canopies, scuppers, downspouts, and metal railings should follow the aesthetic of the building theme.
- 8) The use of concrete is allowed as long as it is part of an overall architectural composition, and it should have a finished architectural expression.
- 9) Façade elements constructed of foam or foam molding are prohibited on the ground floor of buildings and should be avoided overall. If used, they should be well proportioned and constructed to avoid appearing glued to the building.
- 10) Concrete masonry units should only be used if they are fundamental to the building design and have a suitable appearance at the ground floor.
- 11) Signs should be constructed with high quality materials such as metal and architecturally designed.
- 12)

5.12 Windows, Doors, Balconies, and Walls

- 1) The rhythm of windows and entrances should provide interest and engage pedestrians.
- 2) Clear glass should be used on the ground floor of non-residential façades with marginal obstruction from window signs, permanent shades, or interior displays.
- 3) Balconies and bay windows in upper stories are encouraged to enhance activity and provide “eyes on the street.”
- 4) The design, size, type, and location of windows should enhance interior daylight and potentially decrease the size/type of required heating/cooling systems.
- 5) For non-residential storefronts, curtain wall, metal panel, frameless glass porch wall systems, and high quality glass storefront wall systems should be used.
- 6) Installation using a vertical cavity system and reinforced fiber cement panels is acceptable.
- 7) Windows and glass curtain wall systems should be transparent. Highly reflective or very dark glass is not allowed.
- 8) For residential buildings, windows should be of high quality and afford a shadow line and depth. This may be achieved through inset windows with an integral frame or inseting the window into the exterior wall.

5.13 Architectural Lighting

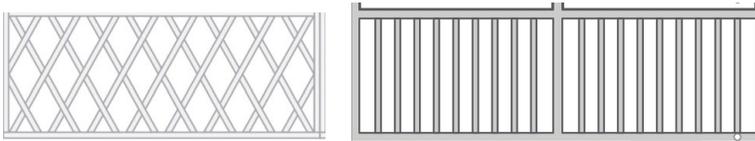
- 1) Lighting should enhance the building's architecture and augment the street and sidewalk experience at night.
- 2) Direct lamp glare from unshielded floodlights is not permitted.
- 3) Lighting that aims light directly into the night sky is prohibited.
- 4) Internal and external storefront lighting should be designed for ground-floor retail and restaurant spaces to augment the pedestrian space and encourage window shopping even when stores are closed.
- 5) Special illumination should be used to highlight main building entrances and add interest to the building façade. Subtle lighting to accent the architecture and special architectural elements (such as distinctive building rooftops) is encouraged.
- 6) Secondary building entrances and parking/loading/service access points should have lighting compatible with the project's lighting to maintain a safe environment around the entire project, especially where pedestrians and other building tenants circulate.
- 7) Warm white light is encouraged. Blinking, flashing, and oscillating lights are prohibited. Colored lights are not encouraged unless they contribute to the theming of commercial areas or establishments. Overly bright or glaring lights should be avoided.
- 8) Automatic timers should be programmed to maximize personal safety at night while conserving energy. They should be reset seasonally to match the flux of dusk/dawn.
- 9) Exterior lighting should be designed and located to not project off-site or onto adjacent uses. This is especially critical with neighboring residential uses.

5.14 Public Art

- 1) Public art shall be developed in the most accessible and visible places and considered in relation to other visual elements and cues (signage and other elements that may impede or heighten its enjoyment).
- 2) Public art shall reflect Chapman Corridor's visual and cultural setting. New installations shall provide a contextual understanding of and be clearly related to the City's overall network of public art.
- 3) Artists shall create sustainable, maintainable works of art that aspire to the highest standards of innovation and aesthetic quality.
- 4) Public art shall be integrated into the project's design at an early stage of development to ensure cohesiveness of site design, architecture, art, landscape, and public space.
- 5) Public art shall be made a condition of approval for all new development at the discretion of the appropriate approval body, including approvals made at the staff level.

5.15 Outdoor Dining

- 1) Outdoor dining facilities such as chairs, tables, fences, planters, and such related furnishings and equipment shall not exceed 60 inches in height and, if permanently installed, must have a transparency of at least 80 percent. Such furnishings under 48 inches in height are not subject to transparency restrictions. Umbrellas, awnings, or other shade covering, plant material, temporary partition, or other movable items used in conjunction with aforementioned chairs, tables, fences, planters, and such related furnishings and equipment may exceed 60 inches in height and are not subject to transparency restrictions.



80 percent transparency examples

- 2) There is no minimum or maximum number of tables or chairs that are permitted pursuant to this section.

6 MOBILITY & INFRASTRUCTURE

6.1 Mobility

Chapman Avenue is an approximately one-mile stretch that runs from the 57 freeway to Placentia Civic Center. The corridor serves as an east-west, auto-oriented thoroughfare that connects the southwestern area of the Placentia to the 57 freeway, Cal State Fullerton, and the City of Fullerton.

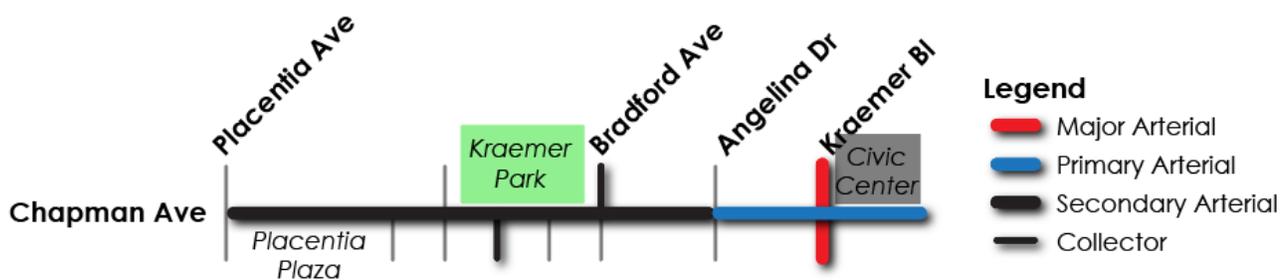
This chapter provides circulation and mobility improvements for Chapman Avenue within the Chapman Avenue Revitalization Plan (CCRP) area. While land use changes and design and development standards focus on the private realm, mobility addresses the public realm to improve accessibility and safety and accommodate users of all ages, including pedestrians, transit passengers, cyclists, and motorists.

6.1.1 Roadway Network

The Orange County Master Plan of Arterial Highways (MPAH, 2017) establishes a countywide surface roadway network intended to provide a guideline for the development of an inter-community arterial highway system. Roadways identified in the City of Placentia’s General Plan Mobility Element (2019) are required to be consistent with roadways in the MPAH.

The MPAH classifies the length of Chapman Avenue as a primary arterial road. The City’s Mobility Element identifies Chapman Avenue as several variations of an arterial. From Placentia Avenue to Angelina Drive it is classified as an existing functional secondary arterial. East of Angelina Drive to Orangethorpe, Chapman Avenue is classified as a primary arterial. Primary and secondary arterial streets are designed for through-vehicular traffic. The roadway network classifications are identified on Figure 6-1.

Figure 6-1: Placentia Mobility Element, Existing Roadway Conditions (2019)

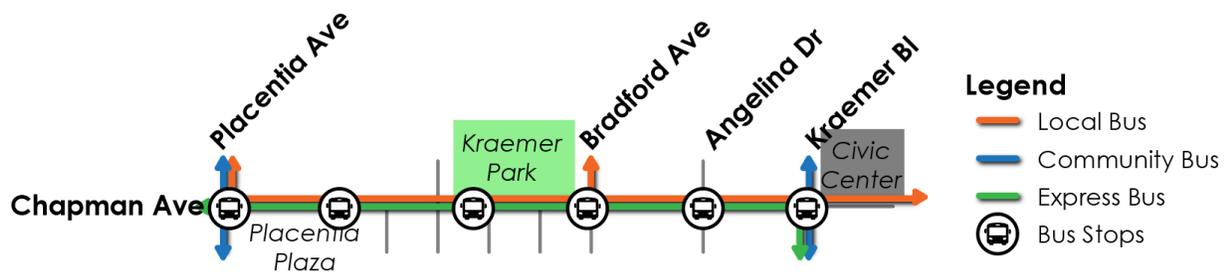


Due to traffic demand and the corridor’s function as a vehicular thoroughfare, implementation of this Plan does not require the addition or removal of vehicular travel lanes. However, the proposed roadway sections have been designed to accommodate bicycle lanes that connect with the City’s existing bicycle network (see Figure 6-3) and keep traffic moving. Existing and proposed street sections are provided later in this chapter on Figures 6-4 to 6-12.

6.1.2 Transit Network

Local, community, and express bus routes run east-west along Chapman Avenue, and north-south along Placentia Avenue, Bradford Avenue, and Kraemer Boulevard (see Figure 6-2). The bus routes are serviced by Orange County Transportation Authority. Bus stops along the corridor typically include signage affixed to a freestanding pole or street light post. Bus-stop enhancements such as seating, bus shelters, pedestrian-scaled lighting, and real-time bus arrival info would greatly enhance the experience for transit users going to and from Chapman Corridor.

Figure 6-2: Existing Transit Network



Existing Bus Stop



Example Bus Shelter

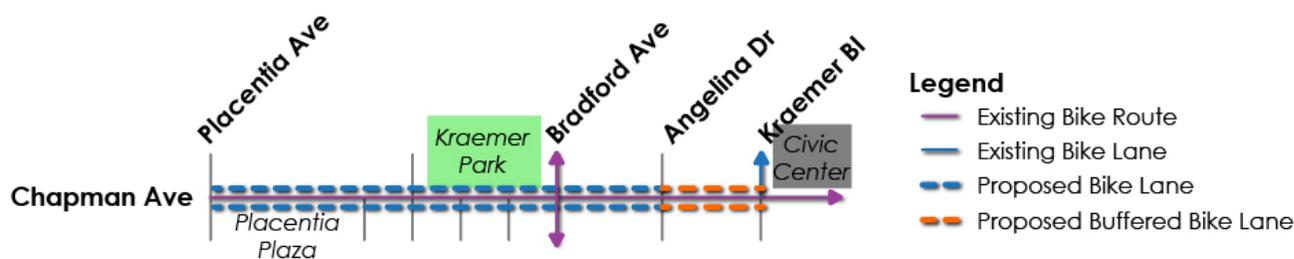
6.1.3 Pedestrian Infrastructure

Chapman Avenue typically includes six- to eight-foot-wide sidewalks on both sides of the corridor. The existing sidewalk widths are adequate for two pedestrians to walk side by side comfortably. Where possible, street trees can provide shade and enhance the pedestrian experience and comfort along the corridor. High-visibility crosswalks would also improve safety at all pedestrian crossings.

6.1.4 Bicycle Network

Chapman Avenue is currently a designated bike route without markings. The auto-oriented nature of the road and high traffic speeds provide an uncomfortable experience for bicyclists. Additionally, the presence of residential driveways and commercial surface parking lots present challenges and unsafe conditions for bicyclists. To improve bicyclists' safety, this Plan includes a continuous eastbound and westbound bike facility from Placentia Avenue to Kraemer Boulevard. A bike lane is proposed from Placentia Avenue to Angelina Drive. Additional curb to curb right-of-way allows for a buffered bike lane from Angelina Drive to Kraemer Boulevard. To accommodate the bike facility, on-street parking is removed on Chapman Avenue across from Kraemer Park. The street reconfiguration does not add or remove travel lanes. Street sections that showcase existing and proposed roadway configurations are shown below in Section 6.2.

Figure 6-3: Existing and Proposed Bicycle Facilities



6.2 Street Sections

This section shows existing and proposed street sections along Chapman Avenue in the plan area. All proposed street sections fit within the existing right-of-way.

6.2.1 Chapman Avenue: Placentia Avenue to Walnut Avenue, Bradford Avenue to Angelina Drive

Chapman Avenue at Placentia Avenue to Walnut Avenue and Bradford Avenue to Angelina Drive feature wide travel lanes and a center median, exemplifying the autocentric nature of the corridor. Lane widths are reduced to accommodate dedicated bike lanes in each direction, shown in Figures 6-5 and 6-6.

Figure 6-4: Chapman Avenue: Placentia Avenue to Walnut Avenue, Bradford Avenue to Angelina Drive Key Map

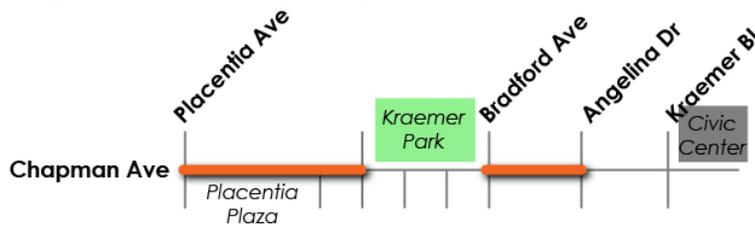


Figure 6-5: Existing Street Section, Chapman Avenue: Placentia Avenue to Walnut Avenue, Bradford Avenue to Angelina Drive Key Map

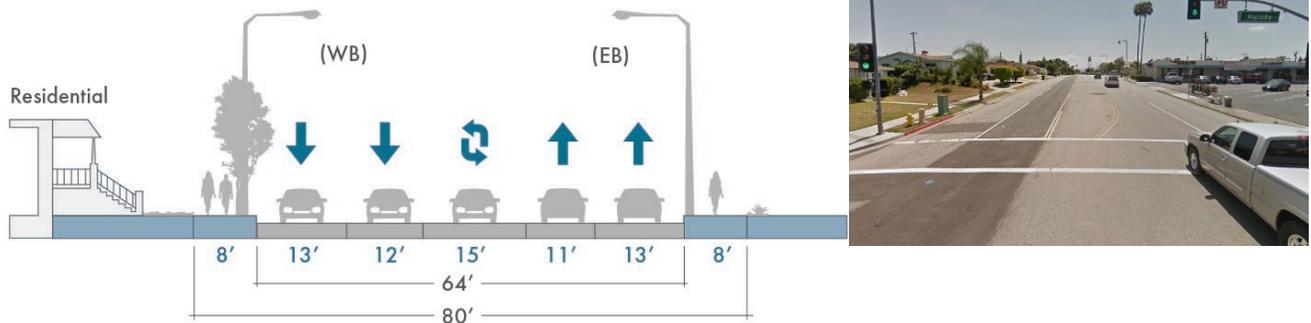
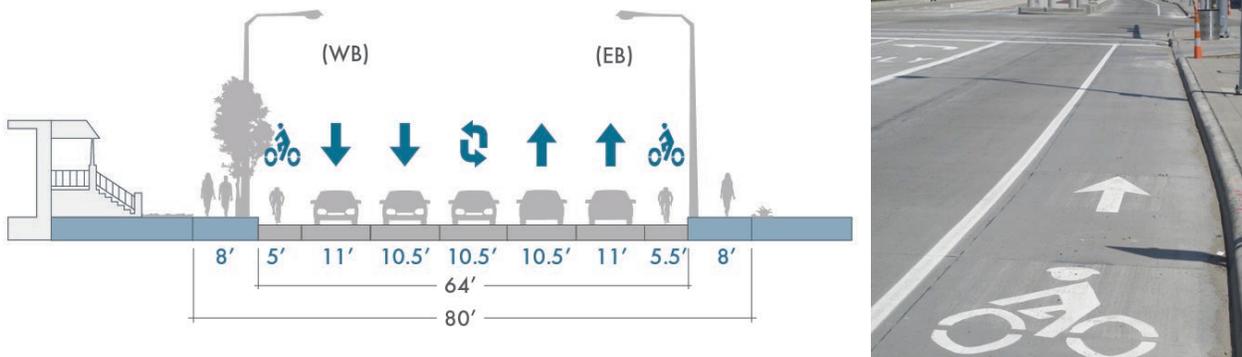


Figure 6-6 Proposed Street Section, Chapman Avenue: Placentia Avenue to Walnut Avenue, Bradford Avenue to Angelina Drive Key Map



6.2.2 Chapman Ave: Walnut Avenue to Bradford Avenue (Kraemer Park)

Proposed improvements to Chapman Avenue between North Walnut Avenue and Bradford Avenue include reconfiguring the lanes to improve bicycle safety and connections to Kraemer Park and the citywide bicycle network. To accommodate an bike lanes, on street parking is removed on the south side of Chapman Avenue in this section. Figures 6-8 and 6-9 show the existing and proposed improvements for this stretch of the corridor.

Figure 6-7: Chapman Avenue East of Bradford Avenue Key Map

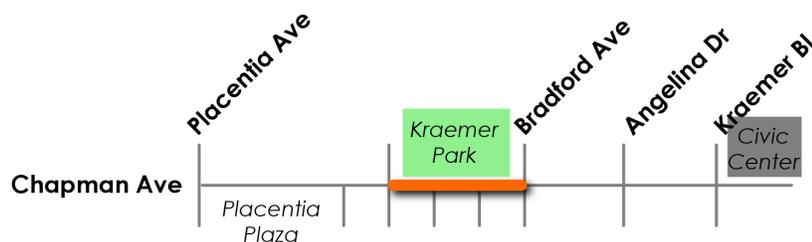


Figure 6-8: Existing Street Section, Chapman Avenue East of Bradford Avenue

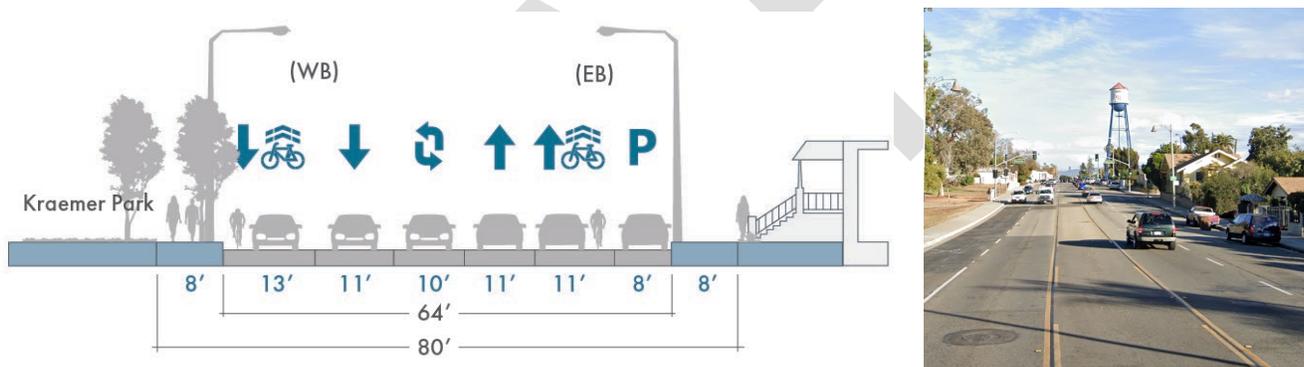
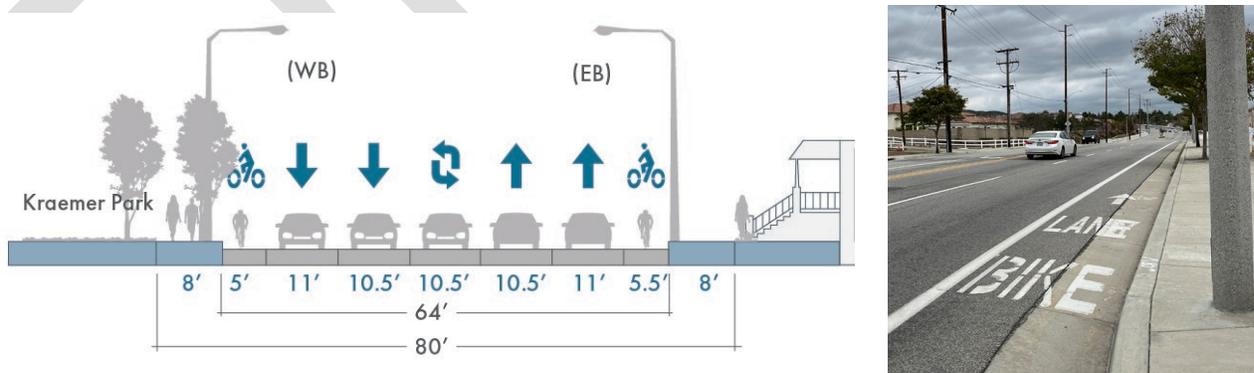


Figure 6-9: Proposed Street Section, Chapman Avenue East of Bradford Avenue



6.2.3 Chapman Avenue: Angelina Drive to Kraemer Boulevard

Chapman Avenue between Angelina Drive and Kraemer Boulevard features a wider right-of-way that can accommodate safer bike facilities. In this section, travel lanes are reduced to accommodate buffered bike lanes.

Figure 6-10: Chapman Avenue East of Bradford Avenue Key Map

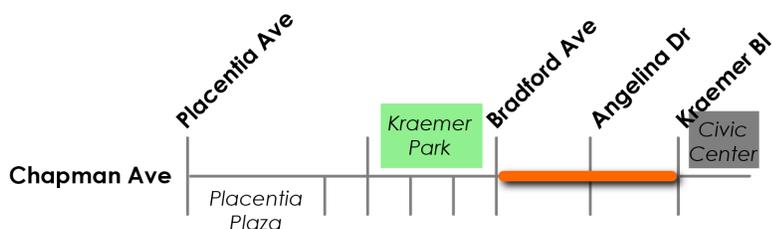


Figure 6-11: Existing Street Section, Chapman Avenue East of Bradford Avenue

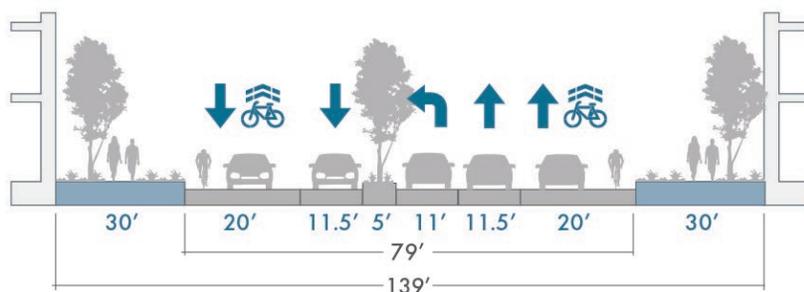
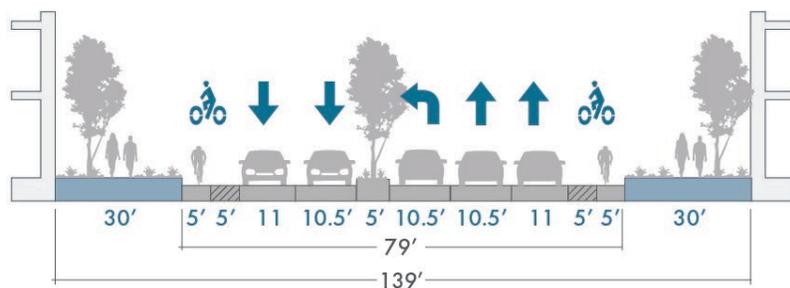


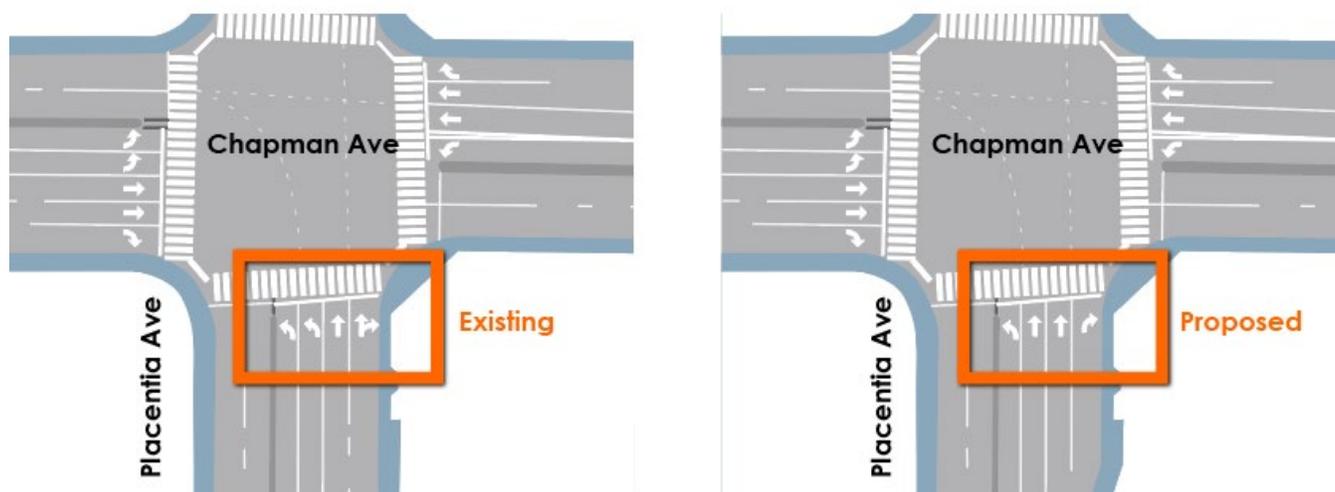
Figure 6-12: Proposed Street Section, Chapman Avenue East of Bradford Avenue



6.2.4 Intersection Improvements

This plan proposes a minor modification to the intersection of Chapman Avenue and Placentia Avenue to accommodate potential traffic impacts as a result of buildout of this plan. The northbound approach of the intersection is recommended to be restriped from two left turn lanes, one through lane, and one through/right turn lane to one left turn lane, two through lanes, and one right turn lane. All improvements would be made within the existing right-of-way. This recommendation is shown on Figure 6-13.

Figure 6-13: Placentia Avenue and Chapman Avenue Intersection Improvements



6.3 Infrastructure

Section forthcoming

7 ADMINISTRATION AND IMPLEMENTATION

7.1 Introduction

This chapter describes the steps and actions to implement and administer the Chapman Corridor Revitalization Plan (CCRP).

7.2 Administration

7.2.1 Applicability

The CCRP was prepared in conformity with the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Section 65450 et seq.; the City of Placentia's General Plan; and the City of Placentia Municipal Code (PMC) and adopted by ordinance.

The CCRP serves as the implementation tool for the zoning for the Plan area. This Plan addresses general provisions, permitted uses, development and design standards, mobility and infrastructure improvements, and design guidelines. Proposed development plans, tentative tract or parcel maps, and any other development approval pertaining to land or property governed by this Plan must be consistent with this Plan. The CCRP supersedes the City of Placentia Municipal Code. Where the CCRP is silent, the PMC shall apply, using the context and objectives of the CCRP as a guide.

7.2.2 Responsibility and Enforcement

The Developmental Services Department is responsible for the overall administration and enforcement of this Plan. The Development Services Director or his/her designee shall be the individual responsible for coordination of review and decision-making and the provision of information regarding the status of all applications and permits for developments.

7.2.3 Severability

If any portion of this CCRP and its regulations are declared invalid or ineffective in whole or in part by a court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Plan.

7.2.4 Interpretation and Use Conformity Determination

In the event of ambiguity concerning the content or application of the Corridor Plan, the ambiguity shall be resolved by the development services director or his/her designee. The development services director or his/her designee may review and determine whether a proposed use that is not otherwise specified in Table 3-2 conforms to the intent of the CCRP. The following shall guide an interpretation:

- The interpretation is consistent with the purposes and intent of the CCRP.
- The interpretation does not alter the intent of the CCRP as defined by the vision, development standards, and design guidelines.

7.2.5 Minor Alterations

The Chapman Corridor Revitalization Plan includes a process to grant minor relief from development standards. The development services director or his/her designee shall have the authority to approve minor alterations to a development application as defined herein. Minor alterations may also be negotiated as part of a development agreement, which may require a developer to provide or finance any public projects deemed agreeable to both the developer and the City Council.

Minor alterations may include:

- Adjustments to development standards by no more than 10 percent (including parking).
- Adjustments to allowable projections and encroachments if consistent with PMC Title 20 Building Codes and Regulations.
- Modifications necessary to comply with final Conditions of Approval or mitigation measures when adopted under subsequent action.
- Addition of information to the CCRP (text or map changes) for purposes of clarification, provided that the additions do not change the intent of the Plan or regulations.
- Adjustments to the alignment, location, and sizing of utilities and facilities, or a change in utility and/or service provider, as approved by the public works department.

Findings:

- In approving a minor alteration, the development services director or his/her designee must make at least one of the following findings:
 - The minor alteration is consistent with the purposes and intent of the CCRP.
 - The minor alteration is consistent with the purposes and intent of the General Plan.
 - The minor alteration will not be detrimental to the public health, safety, and welfare of the existing or future development in the surrounding area.

7.2.6 Amendments to the Specific Plan

Any deviation or relief from the requirements of the CCRP that cannot be accommodated by an Interpretation or Minor Alteration, according to Sections 7.2.4 and 7.2.5, shall require a Plan Amendment. An amendment shall be required if the following is proposed:

- Change in the CCRP boundary.
- An increase or decrease of more than 10 percent in the quantitative development standards.
- An increase in the maximum buildout of residential units and nonresidential square footage (not including any applicable density bonus units).
- A change in use not allowed in the CCRP that was determined (through Section 7.2.4) not to be consistent with the Permitted Use or Conditional Use (Chapter 3 Land Use, Table 3-2, Land Use and Permit Requirements).

An amendment to the CCRP shall be processed as specified in PMC 23.96 and require approval of City Council.

7.2.7 Development Plan Review

Construction of new buildings and exterior additions within the CCRP area shall require that the development services director first makes a finding that the proposed new building or exterior addition is in conformity with both the intent and provisions of this Plan.

The development services director or designee shall have the authority to approve the application for site development approval only if all the following conditions are met:

1. The proposed development is consistent with the permitted uses in Table 3-2, Land Use and Permit Requirements, of this Plan.
2. The proposed development meets or exceeds the criteria established in PMC Section [23.75.020](#) and Chapter 4, Development Standards, of this Plan.
3. There have been attached any other conditions necessary to prevent: (A) detriment to the health, safety or general welfare of the persons residing or working within the neighborhood of the proposed development or within the city, or (B) injurious to the property or improvements within the neighborhood or within the city.
4. The proposed development will be consistent with the latest adopted general plan.
5. Conditions necessary to secure the purposes of this section, including guarantees and evidence of compliance with conditions, are made part of the development approval.

7.2.8 Conditionally Permitted Uses

Conditionally permitted uses established in Table 3-2, Land Use and Permit Requirements, of the CCRP are subject to the approval of a use permit and require a public hearing before the planning commission in

compliance with Chapter 23.87 of the PMC. The planning commission or the City Council Housing, Community, and Economic Development Committee may approve the use permit only if these three conditions are met.

1. The proposed use will not be: (A) detrimental to the general health, safety or general welfare of the persons residing or working within the neighborhood of the proposed development or within the city, or (B) injurious to the property or improvements within the neighborhood or within the city.
2. The proposed use will be consistent with the latest adopted general plan.
3. Conditions necessary to secure the purposes of this section, including guarantees and evidence of compliance with conditions, are made part of the use permit approval

7.2.9 Appeals

Appeals shall be pursuant to PMC 23.75, Development Plan Review, and PMC 23.87, Use Permits. The decision of the development services director shall be final unless appealed to the planning commission. The decision of the planning commission shall be final unless appealed to the city council. The city council or planning commission shall make its own determination as to whether the proposed development conforms to land use regulations and meets the developments standards in the CCRP, and may approve, modify, or disapprove the decision of the planning commission or development services director.

7.2.10 Review Authority

Table 7-1 summarizes the review authority for each type of approval that will or might be required after the CCRP is approved.

| Reviewing Body Designation | Approval Type |
|---|---|
| Development Services Director or his/her Designee | <ul style="list-style-type: none"> • Site Plan Review • Interpretations and Use Conformity • Minor Alterations • Shared Parking Agreements and Parking Management Plans (also reviewed by the Public Works Department) |
| Planning Commission or City Council Housing, Community and Economic Development Committee | <ul style="list-style-type: none"> • Development Plan Review • Tentative Tract Map(s) Recommendation/Tentative Parcel Map(s) Recommendation • Appeal of Development Services Director action • Variance • Conditional Use Permits |
| City Council | <ul style="list-style-type: none"> • Corridor Plan Adoption/Amendment • Tentative Tract Map(s) Approval/Tentative Parcel Map(s) Approval • Environmental Certification • Appeal of Planning Commission Action • Development Agreement Adoption/Amendment |

7.3 Implementation

The CCRP area is expected to undergo sizable growth in housing, employment, commercial retail, and other supporting uses. There are a number of implementation and financing strategies that could or will be applied—both public and private—to realize the vision and potential of the CCRP area. These resources and action items are described below.

7.3.1 CEQA Exemption

The CCRP will be adopted in compliance with the requirements of the California Environmental Quality Act (CEQA) (California Public Resources Code, §§ 21000 et seq.). The City of Placentia finds that the adoption of this plan will not cause a significant impact on the environment and is exempt from CEQA pursuant to CEQA Guidelines Section 15061(b)(3).

Also known as the “general rule” exemption, CEQA Guidelines Section 15061(b)(3) exempts activities that can be seen with certainty to have no possibility for causing a significant effect on the environment. The Guidelines state that “A project is exempt from CEQA if... The activity is covered by the general rule that CEQA applies only to projects which have the potential for causing a significant effect on the environment. Where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment, the activity is not subject to CEQA.”

7.3.2 Phasing

The phasing of new development and revitalization of existing buildings on private properties will occur incrementally, over time as landowners and developers respond to new market opportunities. Redevelopment of opportunity sites, noted in Chapter 3 of this Plan, is expected to occur in the short term (1 to 5 years).

To attract and spur mixed-use residential development in the CCRP area, development incentives for a catalytic project are included in this Plan. The intent of the catalytic project, once approved, is to attract interest and investment in other parts of the corridor. Following the catalytic project, new development or adaptive reuse of existing buildings, such as those in the Single Family Flex zone, will ensue over time in response to market demand and the availability of financing. Improvements along the corridor are identified in Table 7-2, along with a general time frame for when they are expected based on funding.

| Corridor Plan Revitalization Action Items | Time Frame | Primary Responsibility | Potential Funding Sources |
|---|-------------------|-------------------------------|----------------------------------|
| Establish and Adopt CCRP Development Fees | Short (1–5 years) | City | City General Fund |
| Prepare Nexus Study, Create and Adopt Development Impact Fees | Short (1–5 years) | City | City General Fund |
| Approval of Catalyst Project | Short (1–5 years) | Developer, City | Private Funding |
| Redevelopment of Vacant and Underutilized Lots | Short (1–5 years) | Developer | Private Funding |

| Corridor Plan Revitalization Action Items | Time Frame | Primary Responsibility | Potential Funding Sources |
|--|---------------------|-------------------------------|--|
| Chapman Avenue Streetscape Improvement Plan | Short (1–5 years) | City | City, general fund or grant |
| Streetscape Improvements (Street trees, landscaping, lighting) | Medium (5–10 years) | City | Community Facilities District, City, Grant funding |
| Gateway Signage | Medium (5–10 years) | City | Community Facilities District, City, Grant funding |
| Roadway Restriping | Medium (5–10 years) | City | Community Facilities District, City, Grant funding |

7.4 Funding

Several potential funding sources are identified in Table 7-2, these sources are further described below. In general options include grants, private loans, and value capture funding mechanisms. Depending on the project type funding may come from multiple sources.

7.4.1 Community Facilities District

The City intends to require the catalyst site developer and/or the first entitled new development to initiate a Chapman Corridor Community Facilities District (CFD). A Community Facilities District is a special tax district provided by State Law that funds public improvements and ongoing services within an identified area. Generally, these districts fund a wide variety of services, such as parks and open space maintenance, and finance public infrastructure projects. The City currently administers two CFDs in Placentia—CFD 2014-04 (Public Safety Services) and CFD 2018-01 (Transit Oriented Development Maintenance Services).

In addition to City baseline impact fees, Chapman Corridor will require additional public realm improvements, and projects are subject to a Chapman Corridor community facilities district fee that ensures all public sector infrastructure improvements can be provided. In addition to the Chapman Corridor CFD fee, all projects will be required to install public infrastructure and streetscape elements up to the curb face of the public street immediately adjacent to the project, in accordance with the City’s Parkway Design Guide, an adopted Chapman Corridor streetscape plan, and implementation of area gateway signage.

7.4.2 Development Impact Fees

A future Chapman Corridor specific development impact fee program may be initiated in addition to a citywide development impact fee. Development impact fees are a potential funding source for affordable housing, parks, recreational open space, and infrastructure upgrades. These fees, paid by new residential and commercial development projects, may only be used to pay for improvements necessitated by the new development and only in an amount proportionate to the new development’s share of the benefit. The fees can be combined with other funding sources to pay for an improvement that serves both new and existing residents or businesses. A nexus study—which calculates the new increment of development, estimates the portion of an improvement

project attributable to that increment of growth, and allocates the fee among the new development projects by land use—is required by state law for implementation. Cities can, and usually do, have more than one impact fee, with each fee dedicated to an individual category of improvements, such as roads, water, sewer, parks, affordable housing, etc.

7.4.3 Enhanced Infrastructure Financing District

The enhanced infrastructure financing district (EIFD) is a funding mechanism designed to finance a wide array of infrastructure projects with “communitywide significance,” such as parks, transit improvements, and affordable housing.

An EIFD can be created by a city, county, or joint powers authority to fund specific infrastructure and economic development projects as outlined in a financing plan. An EIFD is established with a set boundary, which may include the Plan area or a portion thereof, a larger part of the city, or more than one separate area in the city. If approved by the voters, an EIFD’s financing plan may include tax increment financing, in which the EIFD can collect the property tax revenue from increases in property values associated with new development. An EIFD can collect the tax revenue that would otherwise have gone to the city’s general fund and revenue from any other taxing entity (such as the county general fund, flood control, etc.) except school districts. The tax increment revenue can be used to secure bond funding for EIFD improvements, which may facilitate the new development. EIFDs can also leverage other funding streams, such as assessment revenues, fees, and state and federal grants.

The City of Placentia in partnership with the County of Orange has already established an EIFD near the city’s future Metrolink Station that includes the Old Town Placentia area and Transit Oriented Development Packing House District. The purpose of this district is to create a funding mechanism that can facilitate the construction of public infrastructure improvements in the area. In 2020, the city council established a public financing authority to oversee the creation and adoption of an infrastructure financing plan.

7.4.4 Private Sector Financing

New development in the corridor will largely be financed by private-sector developers. This will include the extension of water, sewer, and stormwater infrastructure; building construction; grading; and other site improvements and amenities. Improvements and adaptive reuse of existing buildings that are privately owned will also be largely financed by the building owners or developers.

The private sector has the widest access to capital to fund new investment in Chapman Corridor, provided that potential investors see opportunities for reasonable returns on investment without undue risk and uncertainty. Other sources of funding, including grants by the state or federal government, can leverage and supplement private investment by supporting reasonable returns on investment and reducing uncertainty and risk. In addition, a local jurisdiction can pursue public financing mechanisms to assist with the costs of certain kinds of public infrastructure improvements to help incentivize economic development within an area. These sources of public financing mechanisms are described further.

7.4.5 Regional and State Grant Funding

Several regional and state agencies may provide grant opportunities that could be utilized to support the revitalization of Chapman Corridor. Some examples include: CalTrans Active Transportation Program; Golden

State Acquisition Fund, administered by the California Housing and Community Development Department; Southern California Association of Governments; and the Infrastructure State Revolving Loan Fund (ISRF), administered by the California Infrastructure and Economic Development Bank.

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